

PLANNING COMMISSION AGENDA

Thursday, October 3, 2024

CITY OF DAYTON, MINNESOTA

12260 So. Diamond Lake Road, Dayton, MN 55327

REGULAR MEETING OF THE PLANNING COMMISSION – 6:30 P.M.

To Participate in the Meeting, please see cityofdaytonmn.com Calendar for Zoom Invitation.

6:30 **1. CALL TO ORDER**

6:30 **2. PLEDGE OF ALLEGIANCE**

(Roll Call)

6:30 **3. ROLE OF THE PLANNING COMMISSION**

The Planning Commission consists of five residents appointed by the City Council. The Commission administers the Comprehensive Plan, Zoning and Subdivision regulations. The Commission conducts Public Hearings and provides recommendations to the City Council. It is the City Council who may approve or deny land use applications.

6:35 **4. APPROVAL OF AGENDA**

6:35 **5. CONSENT AGENDA**

These routine or previously discussed items are enacted with one motion. Note: Commissioners absent from previous meetings may still vote to approve minutes.

A. Planning Commission Minutes, September 5, 2024

6:35 **6. OPEN FORUM**

Public comments are limited to 3 minutes for non-agenda items; state your name and address; No Commission action will be taken, and items will be referred to staff. Group commenters are asked to have one main speaker.

6:40 **7. COUNCIL UPDATE**

A. September 10, 2024

B. September 24, 2024

6:45 **8. PUBLIC HEARINGS**

6:45 A. Approval of a Comprehensive Plan Amendment, Zoning Map Amendment, and Preliminary Plat of Diamond Creek Addition

7:30 **9. NEW BUSINESS**

7:40 A. Resolution Finding that the Acquisition of certain property by the City of Dayton is in compliance with the City's Comprehensive Plan

7:50 B. Discussion of Metropolitan Council, Imagine 2050

The City of Dayton's mission is to promote a thriving community and to provide residents with a safe and pleasant place to live while preserving our rural character, creating connections to our natural resources, and providing customer service that is efficient, fiscally responsible, and responsive.

8:20 **10. NOTICES AND ANNOUNCEMENTS**

A. Staff Updates

8:30 **11. ADJOURNMENT** (Motion to Adjourn)

NOTICE OF QUORUM:

A possible Quorum of City Councilmembers may be present during the Planning Commission meeting, Thursday, October 3, 2024, 6:30 PM.

I. CALL TO ORDER

DeMuth called the September 5, 2024, Planning Commission Meeting to order at 6:30 p.m.

Present: DeMuth, Browen, Crosland, Preisler, and Van Asten

Absent:

Also in Attendance: Mayor, Dennis Fisher; Jon Sevald, Community Development Director; Hayden Stensgard, Associate Planner II

II. PLEDGE OF ALLEGIANCE

III. APPROVAL OF AGENDA

Sevald requested to remove Item VII B; Public Hearing for 11500 Dayton Parkway (CRG The Cubes). Sevald stated that the options presented to Staff were not viable, so the application was pulled.

MOTION by Preisler, second by Crosland, to approve the agenda as amended. The motion carried unanimously.

IV. CONSENT AGENDA

A. Planning Commission Minutes for August 1, 2024

MOTION by Crosland, second by Browen, to approve the consent agenda as presented. The motion carried unanimously.

V. OPEN FORUM

No one present for open forum.

VI. COUNCIL UPDATE

A. August 12, 2024, City Council Meeting

Fisher updated the Commission on Council actions. Fisher stated that there was nothing unexpected in the Financial Quarterly Report. The City Council approved the EDA "document clean-up." Fisher stated that the Cannabis Moratorium was passed and remains valid until January 2025. Two non-profit organizations, Magnus and Heritage Day Festival requested City Council to waive City Fees. The requests were passed.

Regarding traffic control at Dayton Parkway, Fisher stated that the engineering for traffic lights on both sides of the interchange was approved by the City Council.

Fisher stated that the Fee Schedule was amended and engineering on the water trail was approved. Fisher stated that the engineering for the Territorial Road water extension was also approved.

B. August 27, 2024, City Council Meeting

Fisher updated the Commission on Council actions. The City Council discussed the Holland Concept Plan, which centered mainly on Champlin annexing a small portion of Dayton. No one on the City Council was interested in moving forward at this time.

Fisher stated that the park design contractor, Paul Kangus, gave an explanation about what is going on with the schedule. Commission term limits were discussed. Fisher stated that term limits could force the City to pull in more candidates.

Fisher stated that the Extended Business Permit (Home School Co-Op) went through with a limit of 30 students. The Event Center IUP request from Magnus went through on the condition of submitting an Emergency Response Plan. Sevald stated that they have not yet submitted it.

Fisher stated that the parking lot for Central Park was approved for the lowest bid.

VII. PUBLIC HEARINGS

A. Zoning Map Amendment, 11321 Elm Creek Road (Drake)

Stensgard came forward and stated that 11321 Elm Creek Road is currently zoned A-1, Agricultural District, and the request is that the zoning be changed to R-1, Single-Family Residential. The purpose of the rezoning request is related to the proposed demolition of the existing house on site and subsequent construction of a new single-family home. The lot is currently legally nonconforming regarding impervious surface maximum coverage at 10.95% and the Applicant proposes to increase the impervious surface coverage to 14.09%. Stensgard stated that Staff recommends approval.

Browen questioned if there would be public water and sewer on site. Water is coming through a public easement, and there will be a private septic system.

DeMuth invited the Applicant to come forward.

Ray Drake (Drake Construction) of 18130 Territorial Road came forward to answer questions. Drake stated that there is no septic service available on Elm Creek Road.

Additional conversation ensued.

Drake stated that a per test has been completed. The proposed septic system will be a mound system, which is typical for Dayton.

DeMuth opened the Public Hearing at 6:44 p.m.

No one was present for the public hearing.

DeMuth closed the Public Hearing at 6:45 p.m.

Stensgard stated that he received an email a couple of days ago regarding this property. The Westerly property owner has no problems with this application going forward, and they are happy to see something happen with the existing home on site.

Preisler stated that the lot size is consistent with the other residential lots in the area, and it makes a lot of sense to go ahead and approve it.

Van Asten gave her approval. Browen approved. DeMuth noted a zoning map error that needs to be taken care of. Browen wanted to point out that the City should be looking at the small lots that are zoned A-1 and get them re-zoned.

Additional conversation ensued.

MOTION by Browen, second by Preisler, to recommend approval on Zoning Map Amendment, 11321 Elm Creek Road (Drake). The motion carried unanimously.

B. Concept Plan, 11500 Dayton Parkway (CRG – The Cubes)

This item was removed upon adoption of the agenda.

VIII. NEW BUSINESS

A. Concept Plan (Revised), DCM Farms (Dehn)

Sevald came forward and stated that the Applicant is Tom Dehn. The property is located at 14800 113th Avenue North and consists of 91 acres in total. There are 262 proposed lots, of which 59 are single-family homes, and 208 are townhomes. There are 11 acres of commercial/retail space.

Sevald projected an image of the proposed Concept Plan and listed the following as Staff comments and recommendations: 1) Berm and landscape along Fernbrook Lane; 2) Landscape the median along 114th Avenue; 3) Interconnect the trail loop; 4) Improve the 11th Avenue intersection; 5) Alignment of intersections; 6) Spacing between intersections of 660 linear feet; 7) Create an alley for the existing homes; 8) Secure the property needed for a roundabout at 117th Avenue; 9) Retail space should be pedestrian-oriented; and 10) Iconic signage and architecture should reflect the rural feel associated with Dayton.

DeMuth asked if the required 660 linear feet remain if the development is set up as a right-in/right-out only development. Sevald was unsure. DeMuth drew attention to the portion of the projected Concept Plan that is not part of the plan.

Brown asked for additional clarification of an area that is part of another development.

Tom Dehn, Applicant, of 11261 Fernbrook Lane came forward to answer questions. Dehn stated that the commercial/retail area is behind Dehn' Country Manor.

Brown suggested realignment of the roads in the commercial/retail area.

Dehn plans to begin by building homes and has already spoken with a potential daycare tenant for the commercial/retail space. Dehn intends to court potential tenants as the residential area is being built.

Additional conversation ensued.

Brown raised concern about having 80,000 square feet of commercial/retail space and too few parking stalls. Brown suggested that a three-story multifamily unit would fit nicely close to Dehn's Country Manor. Preisler agreed.

Additional conversation ensued.

There was debate over whether an alley would be functional for the existing homes. The alley would not be constructed, but the easement will be provided for a future alley, should it become necessary to have one.

Dehn is planning on a two-year phasing for the project.

Additional conversation regarding siding and paint ensued. The conversation shifted to lot size, setbacks, and driveway length and then shifted to potential use for the 11 acres that are currently planned for commercial/retail use.

Dehn stated that the residents expressed opposition to density in the area. Preisler stated that the area is perfect for density.

Crosland stated that service businesses would be a good fit too.

Van Asten stated that she understands the concerns of the neighbors, but the City needs some high-density housing, and the buffering is a good solution.

Crosland made a few cosmetic suggestions to build character within the neighborhood.

DeMuth suggested making a connection to the main road by trail or path from the cul-de-sac in the lower left corner of the drawing.

Preisler asked what's going on in the southeast quadrant of the park. The answer is disc golf.

Browen does not like the 40–45-foot lots around the pond and suggested tying a road in for the existing single-family homes.

Additional conversation ensued.

Preisler stated that the roundabout at 113th Avenue is going to require some County participation.

DeMuth opened the floor for public comment at 7:56 p.m.

A gentleman came forward, and Preisler asked him if he and all of his neighbors were notified. The gentleman stated that no one was notified. Sevald clarified that this is not a public hearing. DeMuth stated that he is re-

opening public comment. DeMuth asked the gentleman to state his name and address.

Lee Hallquist of 11471 Fernbrook Lane stated that the neighbors do not want a gas station with 24/7 lights on in front of their homes. Hallquist stated that when you cram a bunch of people together, you're looking for problems. Hallquist is not in favor of high-density housing.

Hallquist complained that he currently pumps water approximately every six minutes, and he believes the City is to blame because the City approved the project that caused the problems. Hallquist does not like the idea of a road going behind his house.

Dehn stated that he met with the neighbors, Sevald, and Doud on a few occasions and the meetings were helpful.

Van Asten thanked Dehn for his efforts and stated that often developers just don't care about the impact that their projects have on the community.

Hallquist is not happy with the construction hours of operation and believes that five nine-hour days should be adequate.

The conversation shifted to grading.

Stensgard stated that he received a comment via Zoom from Aaron Wares, of 14536 112th Avenue asking if there could be an additional buffer between the commercial area. Wares likes the idea of a landscaped median.

Preisler asked if the development would be covered by an HOA. The answer is yes.

DeMuth closed public comment at 8:16 p.m.

B. Discussion of Ordinance Amendment, Cannabis Business

Stensgard came forward to lead the discussion concerning the adult use of cannabis. Stensgard stated that in 2023, the Minnesota Legislature legalized adult use cannabis Statewide. The State created 16 license classifications for cannabis-related businesses to obtain prior to conducting operations. The State gave zoning authority to local governments, providing them the opportunity to regulate business in their communities specific to time, place, and manner. The State requires that all licenses be allowed within a local jurisdiction in some capacity. Cities cannot prohibit any of the

State identified licenses. The purpose of the moratorium is to allow the City an opportunity to adjust zoning.

Stensgard explained that Staff would like input from the Planning Commission, and some considerations were projected for the Planning Commission as a starting point. One consideration is that the business would operate solely within the enclosed business space without any possibility of outside storage.

Another consideration is that retail cannabis store locations would be subjected to the same zoning ordinances as liquor stores.

Stensgard stated that buffering should be considered for all cannabis operations. The Planning Commission should consider if home occupation for lower-potency hemp manufacturing is acceptable. Stensgard stated that Dayton is required to have at least one cannabis retail store.

There was a consensus to remove the capacity for cannabis cultivation from the GMU-5 area.

Additional conversation ensued regarding the standard size of a cultivation building.

Stensgard stated that an indoor cultivation building is limited to not more than 30,000 square feet according to State Statute. Stensgard stated that there are certain State licenses that allow a business to cultivate, process, and sell at the same location.

Discussion ensued regarding the nuisance factor of the odor of cannabis smoke. The conversation shifted to the delivery of cannabis and then to buffer requirements. There was a brief discussion about limiting the hours of operation for sales of cannabis.

Browen is concerned about mitigating the smell for neighbors when cannabis is smoked at home.

IX. NOTICES AND ANNOUNCEMENTS

A. Staff Updates

Sevald stated that the Master Plan Open House will be held Tuesday, September 17, 2024, at the Community Center from 6:30 p.m. until 8:00 p.m. and all the Planning Commission Members are encouraged to attend.

Sevald anticipates that the following items will be on the October Planning Commission agenda: 1) Parkway Neighborhood; 2) Dayton Creek; and 3) Fisher Farms Event Center.

Sevald stated that the EAW for the Dayton Parkway Neighborhood will be going to City Council on September 24, 2024.

Sevald stated that the Met Council Imagine 2050 policy statements will be going to City Council on September 24, 2024.

X. ADJOURNMENT

MOTION by Crosland to adjourn, Preisler seconded the motion. Motion carried unanimously. The meeting adjourned at 9:11 p.m.

Respectfully submitted,

Sandra Major, Recording Secretary
TimeSaver Off Site Secretarial, Inc

ITEM:

Approval of Comprehensive Plan Amendment, Zoning Map Amendment, Preliminary Plat, and Final Plat of Diamond Creek Addition

APPLICANT/PRESENTERS:

Ken Streeter, Streeter Companies
Viki VanDell, WSB

PREPARED BY:

Jon Sevald, Community Development Director

BACKGROUND/OVERVIEW:

The proposed Dayton Creek Addition consists of right-of-way and 11 outlots (4 outlots consist of wetlands). The outlots are intended to be subdivided by others after a known user has been identified. Outlots are non-buildable as-is. Staff's review is primarily focused on the streets and associated stormwater management.

A Concept Plan was reviewed by the Planning Commission on November 3, 2022. The Concept Plan included multi-family and commercial uses on both the north and south portions of the project. The Commission encouraged the Applicant to work with the north landlocked parcel (Brockton Rush Creek Partners) to extend the right-of-way to their property.

A Concept Plan was reviewed by the City Council on November 22, 2022. The Applicant discussed the need for cost participation by Brockton Rush Creek Partners to extend the road and utilities to their property. Additional discussion occurred regarding buffering and the desire for a medical clinic.

Further discussion occurred at the September 12, 2023 City Council meeting. The Applicant requested;

1. That the Council not require the north ROW to be extended to the north property line (Council was agreeable).
2. That outlots containing wetlands not be required to include wetland buffers (e.g. wetland buffer would extend outside the outlots into the future buildable lots) (Council was agreeable).
3. That 60' ROW be allowed instead of 80' on the south cul-de-sac (Council was agreeable, assuming additional ROW could be required at the time the Outlots are re-platted).

Comprehensive Plan Amendment

The 2040 Comprehensive Plan guides the properties as Mixed Use (60% residential, 40% commercial/retail/light industrial), with a density of 12-20 dwelling units per net acre. Portions of the property within the Floodplain are guided Greenway Overlay.¹

¹ 2040 Comprehensive Plan, Figure 3: Future Land Use Map, Land Use; Table 3 Land Use Categories

Properties southwest of I-94 are within the “2020” sewer staging plan.² It is the intent that the Staging Plan be amended from “2020” to “Current”. After the City Council Approves the amendment, the amendment must be reviewed by the Metcouncil, who may authorize the City to put the amendment into effect.

Zoning Map Amendment

The properties are zoned A-1 Agricultural, and guided GMU-5 General Mixed Use: Southwest Mixed-Use. The intent of the GMU-5 district is *“to provide an appropriate location to allow a diverse mix of compatible uses including high density residential, commercial, office, and employment driven industrial related uses.”*³

Preliminary Plat

All development within the GMU-5 district shall be through a Planned Unit Development (PUD).⁴

The Preliminary Plat includes street right-of-way (ROW) and 11 outlots. It is the Applicant’s intent to construct the street and market the outlots for development by others. Future developers will be required to re-plat the outlots as a PUD, and to obtain applicable land use approvals.

Final Plat

Final Plats are not required to be reviewed by the Planning Commission unless substantially different than the approved Preliminary Plat. The Final Plat submitted September 25, 2024 is inconsistent with the Preliminary Plat, dated September 25, 2024 (north ROW does not match). Staff needs sufficient time to review prior to Final Plat approval.

The City’s intent is that Approval of the Final Plat includes approval of a Development Agreement (DA), which requires known fees, escrows, and letter of credit amounts based on estimated construction costs. These costs are unknown at this time.

CRITICAL ISSUES:

The City Engineer’s review, dated September 4, 2024 and Applicant’s response, received September 23, 2024 and revised plan set dated September 25, 2024 include items that are unresolved. Major items include:

1. The Preliminary Plat includes extension of the north road (Dayton Creek Road) to north property line, whereas the Final Plat does not.
2. The south road (Dayton Creek Circle) is shown at 50’ ROW width, whereas the City Council agreed to reduce ROW from 80’ to 60’ (not 50’).

² 2040 Comprehensive Plan, Figure 4: 2040 Staging Plan.

³ City Code 1001.065, Subd 5(1) (Purpose)

⁴ City Code 1001.065, Subd 5(3) (Planned Unit Development Required).

PLANNING COMMISSION MEETING

3. A temporary emergency access easement is needed between the south cul-de-sac (Dayton Creek Circle) and Brockton Lane to service all properties south of Dayton Parkway, including the proposed Parkway Neighborhood (650 units). At the time Outlot H is re-platted, the emergency access can be adjusted to accommodate that project, and a permanent easement dedicated.
4. Three Rivers Park trail is to be moved from the east side of Dayton Creek Road to the west side.
5. Grading & Stormwater plans need to be updated to complete Staff's review.

60/120-DAY RULE (IF APPLICABLE):

| | 60-Days | 120-Days |
|------------------------------|--------------|--------------------------|
| Comprehensive Plan Amendment | July 1, 2023 | Sep 1, 2023 ⁵ |
| Zoning Map Amendment | July 1, 2023 | Sep 1, 2023 |
| Preliminary Plat | July 1, 2023 | Sep 1, 2023 |
| Final Plat | - | - |

RELATIONSHIP TO COUNCIL GOALS:

Build Quality Infrastructure
Planning Ahead to Manage Thoughtful Development
Create a Sought After Community

ROLE OF PLANNING COMMISSION:

Conduct a Public Hearing. Motion to Recommend Approval of an Ordinance Amending the Zoning Map. Motion to Recommend Approval of a Resolution Approving a Comprehensive Plan Amendment; and Preliminary Plat.

RECOMMENDATION:

1. Staff recommends Approval of the Comprehensive Plan Amendment.
2. Staff recommends Approval of the Zoning Map Amendment.
3. Staff recommends Approval of the Preliminary Plat pending satisfactory compliance with the City Engineer's letter, dated September 4, 2024 prior to City Council approval of the Preliminary Plat (tentatively October 22, 2024). Normally, Staff would recommend Tabling action until corrections are made (corrections were submitted September 25, 2024. Staff has not had sufficient time to review). In this case, Tabling action would result in a violation of the 60-Day Rule, and subsequent Approval due to lack of Action.

A Public Hearing Notice was published by THE PRESS on September 19, 2024 and mailed to property owners within 500' of the subject property.

⁵ Applications for Comprehensive Plan amendment, Zoning Map amendment, and Preliminary Plat were submitted May 1, 2023. Subsequent extensions of the City's review were granted by the applicant, expiring November 1, 2024 (email from Applicant, August 12, 2024). Such extensions were due in part to mandatory environmental reviews by US Army Corps of Engineers, and Minnesota Board of Water & Soil Resources, completed June 13, 2024.

ATTACHMENT(S):

Aerial Photo

Zoning Map

Future Land Use Plan

2040 Staging Plan

Engineering Comments (with applicant's response), Sep 23, 2024

Ordinance 2024-__

Resolution __-2024

Concept Plan, Aug 16, 2022

Plan Set



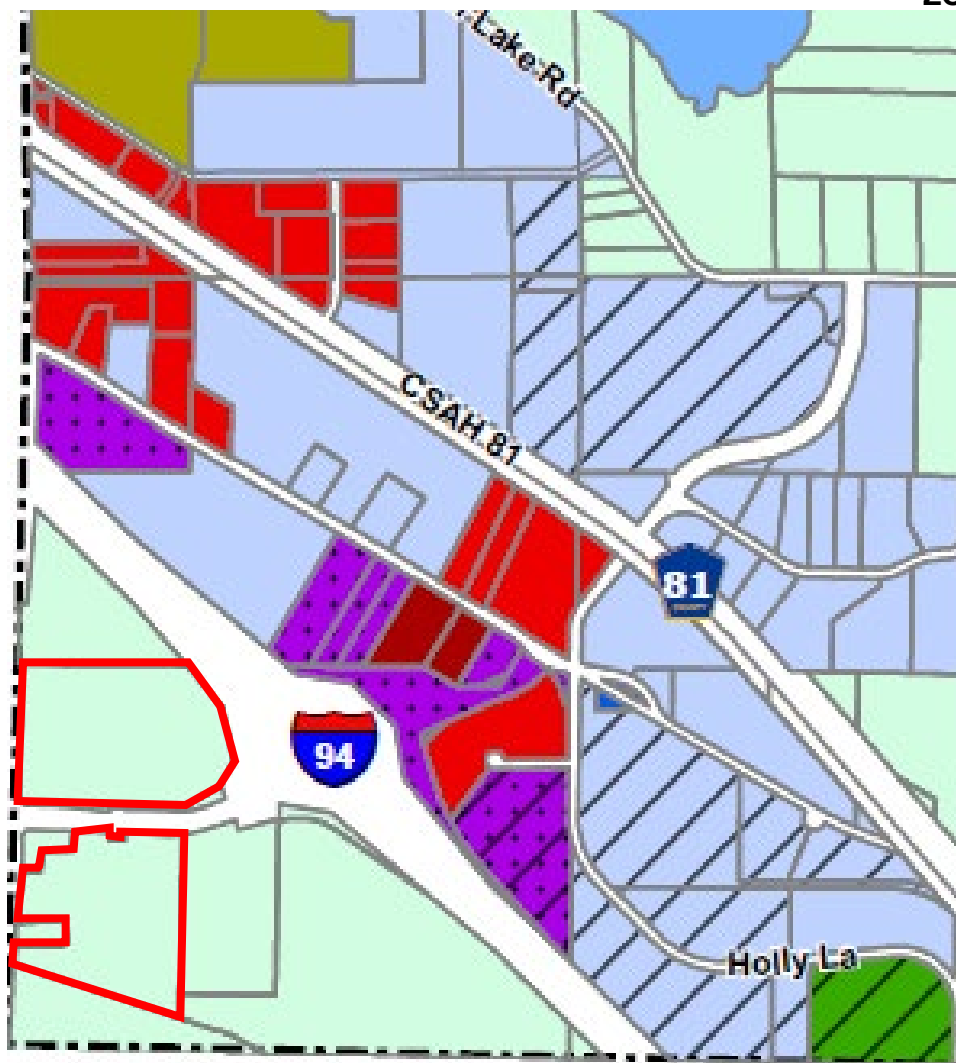
Dayton Creek Addition, Outlot C, looking north along Dayton Parkway (photo Sep 27, 2024)



Dayton Creek Addition, south side of Dayton Parkway & Dayton Creek Road, panoramic view looking west (Sep 27, 2024).



ZONING MAP

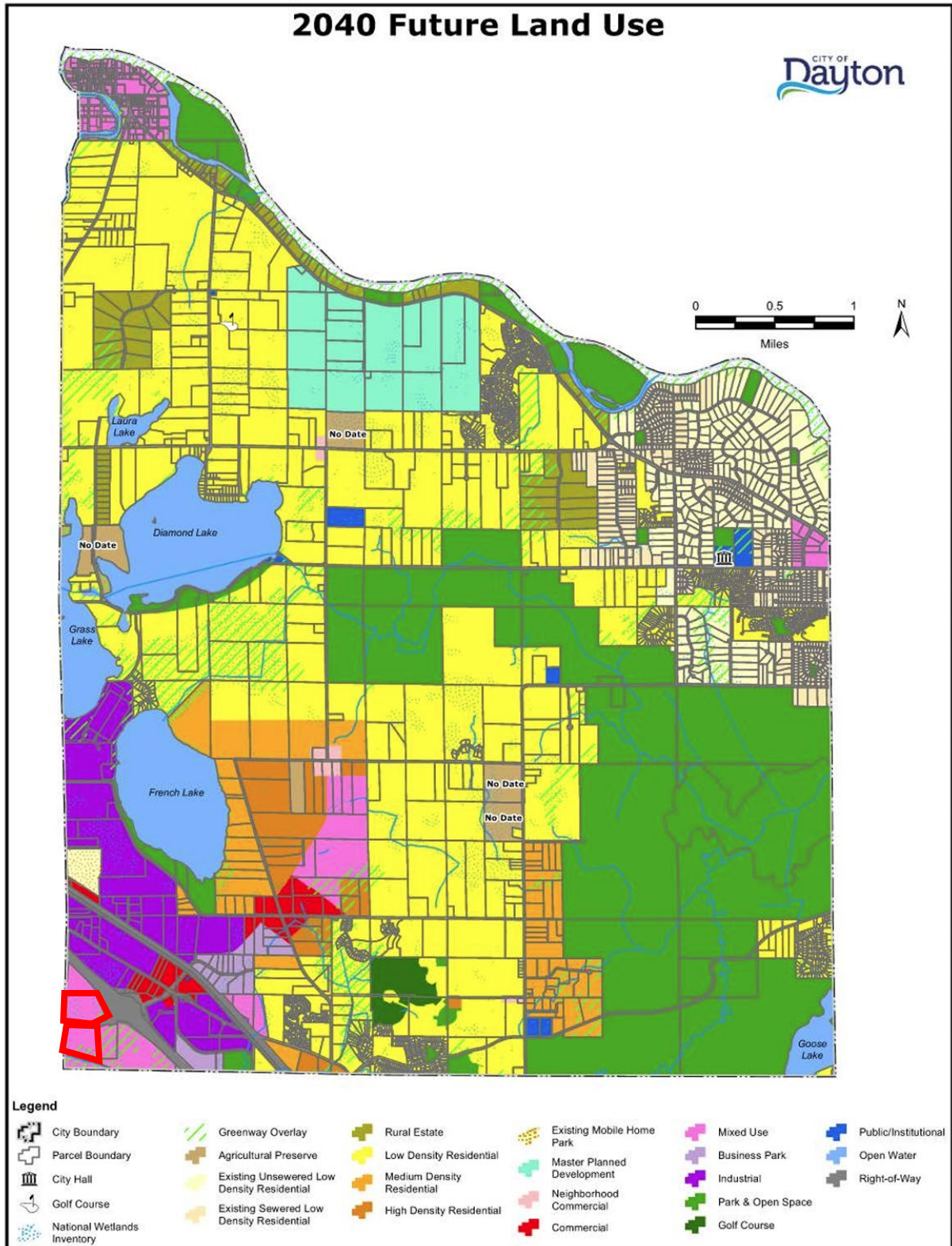


Legend

| | |
|-------------------------------------|---|
| A-1 Agricultural District | R-2 Single Family District (90,000 Sf, Unsewered) |
| A-2 Agricultural District | R-3 Single Family and Attached Residential |
| B-2 Neighborhood Business District | R-E Single Family District (5 Ac, Unsewered) |
| B-3 General Business District | R-M Medium Density Residential District |
| B-4 Commercial/ Industrial District | R-MH Mobile Home District |
| B-P Business Park District | R-O Old Village Residential |
| ES Essential Service District | S-A Special Agriculture District |
| G-MU-4 Balsam Lane | GMU-3 Historic Village |
| I-1 Light Industrial District | City Boundary |
| P-R Public Recreation District | PUD |
| R-1 Single Family District | County Parcels |
| R-1A Single Family Residential | |

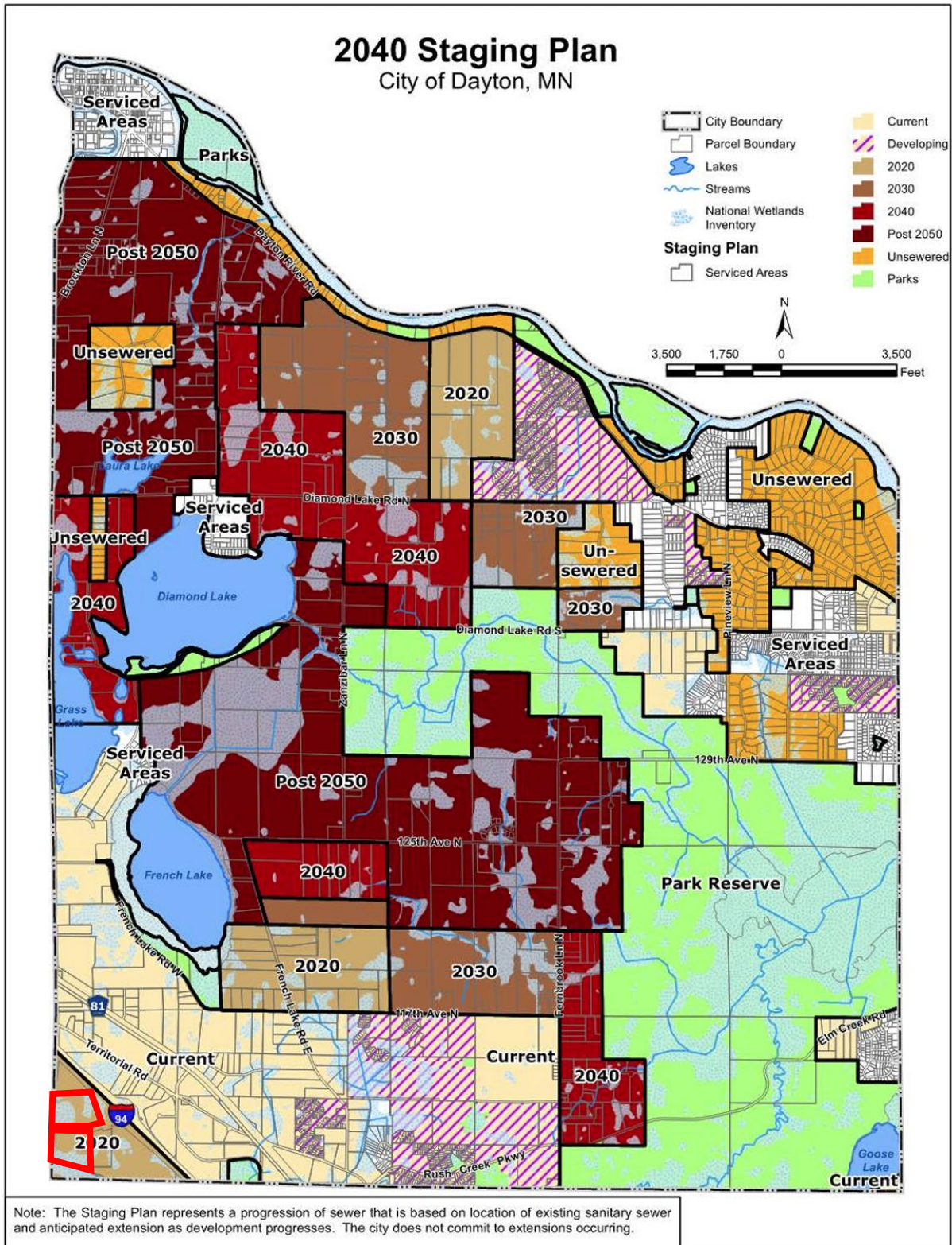
FUTURE LAND USE MAP

Figure 3: Future Land Use Map



2040 STAGING PLAN

Figure 4: 2040 Staging Plan



CADD QUALIFICATION

CADD files prepared by the Consultant for this project are instruments of the Consultant professional services for use solely with respect to this project. These CADD files shall not be used on other projects, for additions to this project, or for completion of this project by others without written approval by the Consultant. With the Consultant's approval, others may be permitted to obtain copies of the CADD drawing files for information and reference only. All intentional or unintentional revisions, additions, or deletions to these CADD files shall be made at the full risk of that party making such revisions, additions or deletions and that party shall hold harmless and indemnify the Consultant from any & all responsibilities, claims, and liabilities.

SUBMITTAL/REVISIONS

DATE _____

PROFESSIONAL SIGNATURE

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

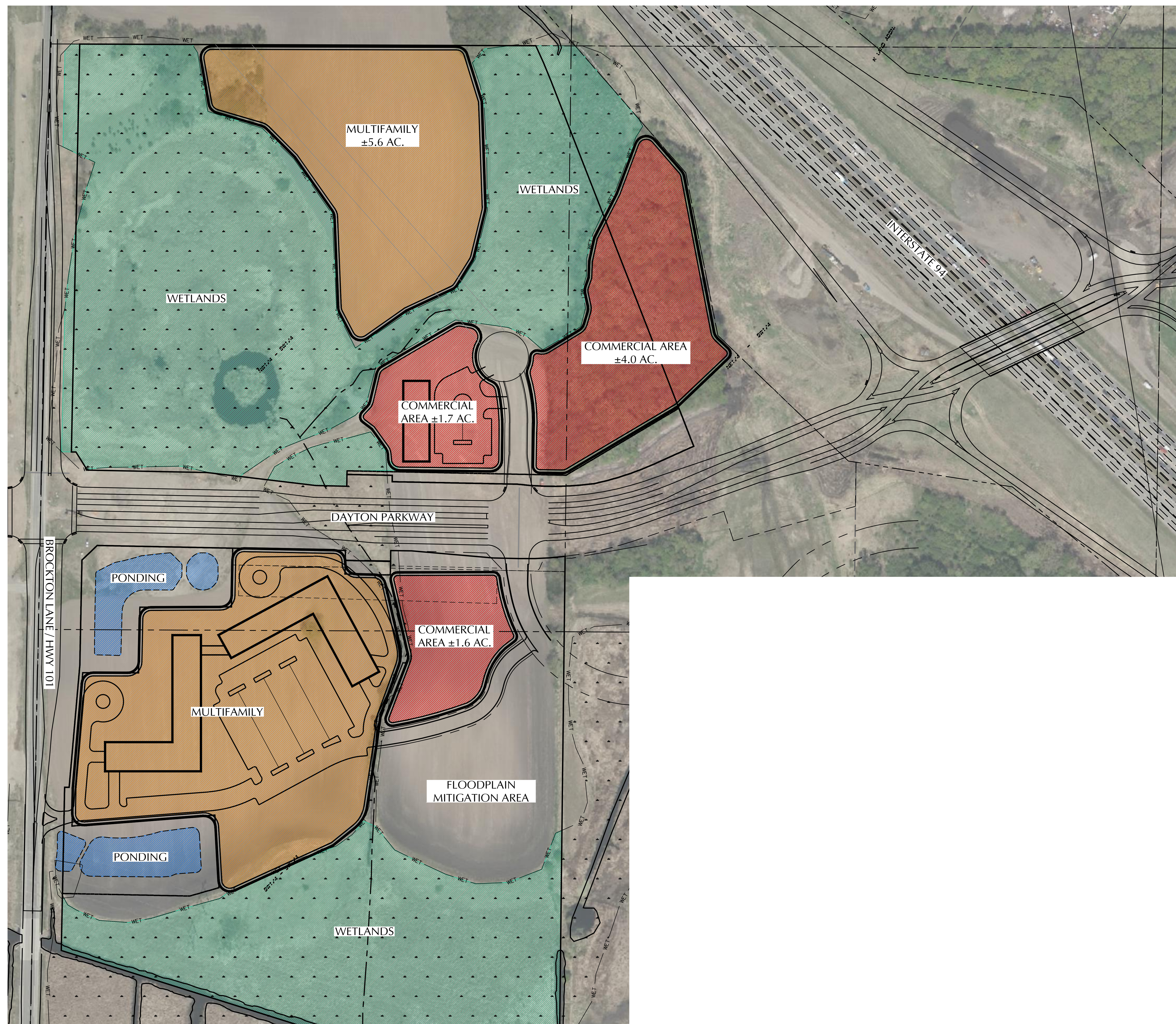
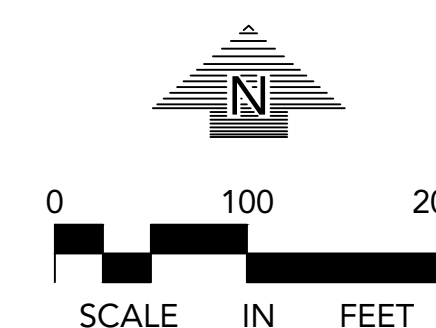
Vicki J. Van Dell - PE
License No. 41352
Date

QUALITY CONTROL

| | |
|--------------------|----------|
| Locuks Project No. | 16158.00 |
| Project Lead | VJV |
| Drawn By | CEF |
| Checked By | VJV |
| Review Date | 08/16/22 |

SHEET INDEX

SHEET



**CITY OF DAYTON
COUNTIES OF HENNEPIN AND WRIGHT
STATE OF MINNESOTA**

ORDINANCE No. 2024-__

**AN ORDINANCE TO AMEND THE DAYTON ZONING ORDINANCE BY AMENDING
ZONING DISTRICTS THEREIN**

THE CITY COUNCIL OF THE CITY OF DAYTON DOES ORDAIN:

SECTION 1. AMENDMENT. The zoning classification of the properties described in Section 2, as shown on the Zoning Map referred to in Section 1001.04, Subd 4, subd. 2 of the Dayton Zoning and Subdivision Code, is hereby amended from A-1 Agricultural to GMU-5 General Mixed Use – Southwest Mixed Use.

SECTION 2. PROPERTY DESCRIPTION.

PID: 31-120-22-32-0014

Outlot A, Brockton Crossing, Hennepin County, Minnesota, except that part described as follows:

That part of said Outlot A described as beginning at the southeast corner of said Outlot A; thence North 21 degrees 20 minutes 58 seconds West, assumed bearing, along the easterly line of said Outlot A a distance of 90.06 feet; thence South 59 degrees 16 minutes 44 seconds West a distance of 289.57 feet; thence North 88 degrees 50 minutes 39 seconds West a distance of 483.55 feet; thence South 01 degree 09 minutes 21 seconds West a distance of 8.00 feet to the southerly line of said Outlot A; thence easterly, southerly, easterly and northeasterly along the southerly lines of said Outlot A to the point of beginning.

AND

That part of Dayton Industrial Boulevard as dedicated on the plat of Brockton Crossing, according to the recorded plat thereof, Hennepin County, Minnesota, which lies northwesterly of the following described line: Commencing at the most easterly corner of Outlot A of said plat; thence North 21 degrees 20 minutes 58 seconds West, along the northeasterly line of said Outlot A a distance of 90.05 feet to the point of beginning of the line to be described; thence North 28 degrees 12 minutes 41 seconds East a distance of 182.06 feet to the northeasterly line of said Dayton Industrial Boulevard and there terminating.

PID: 31-120-22-33-0009

Outlot B, Brockton Crossing, Hennepin County, Minnesota, except those parts described as follows: That part of said Outlot B described as commencing at the southwest corner of said Outlot B; thence North 01 degree 09 minutes 19 seconds East, assumed bearing, along the west line of said Outlot B a distance of 126.07 feet to the point of beginning; thence South 88 degrees 50 minutes 41 seconds East a distance of 281.06 feet; thence North 74 degrees 03 minutes 42 seconds East a distance of 66.90 feet; thence North 03 degrees 14 minutes 45 seconds West a distance of 160.69 feet; thence South 85 degrees 36 minutes 02 seconds West, a distance of 316.15 feet; thence North 01 degree 09 minutes 19 seconds East a distance of 85.08 feet; thence North 04 degrees 58 minutes 11 seconds East a distance of 254.52 feet; thence South 89 degrees 45 minutes 01 seconds East a distance of 123.08 feet; thence North 01 degrees 09 minutes 21

seconds East a distance of 145.00 feet; thence North 89 degrees 45 minutes 01 seconds East a distance of 202.25 feet; thence North 01 degrees 09 minutes 21 seconds East a distance of 131.21 feet to the northerly line of said Outlot B; thence westerly, southwesterly, southerly, westerly and southerly along the northerly, northwesterly and westerly lines of said Outlot B to the point of beginning. And except that part of said Outlot B described as commencing at the most northerly northwest corner of said Outlot B; thence South 88 degrees 50 minutes 39 seconds East, along the northerly line of said Outlot B, a distance of 548.20 feet to an angle point in said northerly line; thence South 01 degree 09 minutes 21 seconds West, along said northerly line, a distance of 22.00 feet to an angle point in said northerly line and the point of beginning; thence South 88 degrees 50 minutes 39 seconds East, along the northerly line of said Outlot B, a distance of 100.00 feet to an angle point in said northerly line; thence South 01 degree 09 minutes 21 seconds West a distance of 30.00 feet; thence North 88 degrees 50 minutes 39 seconds West a distance of 100.00 feet to the intersection with a line bearing South 01 degree 09 minutes 21 seconds West from the point of beginning; thence North 01 degree 09 minutes 21 seconds East a distance of 30.00 feet to the point of beginning.

SECTION 3. EFFECTIVE DATE. This Ordinance shall be in full force and effect from and after its passage.

Adopted this 22nd day of October, 2024, by the City of Dayton.

Mayor, Dennis Fisher

ATTEST

City Clerk, Amy Benting

Motion by Councilmember _____, *Second by Councilmember* _____.
The Motion passes.

Published in THE PRESS on _____.

**CITY OF DAYTON
COUNTIES OF HENNEPIN AND WRIGHT
STATE OF MINNESOTA**

RESOLUTION No. __-2024

**APPROVAL OF COMPREHENSIVE PLAN AMENDMENT TO AMEND THE 2040 STAGING
PLAN FROM “2020” TO “CURRENT”; AND, ZONING MAP AMENDMENT, FROM A-1
AGRICLUTURAL TO GMU-5 GENERAL MIXED-USE DISTRICT-5 SOUTHWEST MIXED
USE; AND PRELIMINARY PLAT OF DAYTON CREEK ADDITION**

WHEREAS, Schany Group, LLC (Applicant) has applied for a Comprehensive Plan Amendment, Zoning Map Amendment, and Preliminary Plat approval of Dayton Creek Addition, consisting of 11 outlots; and,

WHEREAS, the unaddressed property is generally located at the northeast corner and southeast corner of Dayton Parkway and Brockton Lane, and is legally described as:

PID: 31-120-22-32-0014

Outlot A, Brockton Crossing, Hennepin County, Minnesota, except that part described as follows:

That part of said Outlot A described as beginning at the southeast corner of said Outlot A; thence North 21 degrees 20 minutes 58 seconds West, assumed bearing, along the easterly line of said Outlot A a distance of 90.06 feet; thence South 59 degrees 16 minutes 44 seconds West a distance of 289.57 feet; thence North 88 degrees 50 minutes 39 seconds West a distance of 483.55 feet; thence South 01 degree 09 minutes 21 seconds West a distance of 8.00 feet to the southerly line of said Outlot A; thence easterly, southerly, easterly and northeasterly along the southerly lines of said Outlot A to the point of beginning.

AND

That part of Dayton Industrial Boulevard as dedicated on the plat of Brockton Crossing, according to the recorded plat thereof, Hennepin County, Minnesota, which lies northwesterly of the following described line: Commencing at the most easterly corner of Outlot A of said plat; thence North 21 degrees 20 minutes 58 seconds West, along the northeasterly line of said Outlot A a distance of 90.05 feet to the point of beginning of the line to be described; thence North 28 degrees 12 minutes 41 seconds East a distance of 182.06 feet to the northeasterly line of said Dayton Industrial Boulevard and there terminating.

PID: 31-120-22-33-0009

Outlot B, Brockton Crossing, Hennepin County, Minnesota, except those parts described as follows: That part of said Outlot B described as commencing at the southwest corner of said Outlot B; thence North 01 degree 09 minutes 19 seconds East, assumed bearing, along the west line of said Outlot B a distance of 126.07 feet to the point of beginning; thence South 88 degrees 50 minutes 41 seconds East a distance of 281.06 feet; thence North 74 degrees 03 minutes 42

seconds East a distance of 66.90 feet; thence North 03 degrees 14 minutes 45 seconds West a distance of 160.69 feet; thence South 85 degrees 36 minutes 02 seconds West, a distance of 316.15 feet; thence North 01 degree 09 minutes 19 seconds East a distance of 85.08 feet; thence North 04 degrees 58 minutes 11 seconds East a distance of 254.52 feet; thence South 89 degrees 45 minutes 01 seconds East a distance of 123.08 feet; thence North 01 degrees 09 minutes 21 seconds East a distance of 145.00 feet; thence North 89 degrees 45 minutes 01 seconds East a distance of 202.25 feet; thence North 01 degrees 09 minutes 21 seconds East a distance of 131.21 feet to the northerly line of said Outlot B; thence westerly, southwesterly, southerly, westerly and southerly along the northerly, northwesterly and westerly lines of said Outlot B to the point of beginning. And except that part of said Outlot B described as commencing at the most northerly northwest corner of said Outlot B; thence South 88 degrees 50 minutes 39 seconds East, along the northerly line of said Outlot B, a distance of 548.20 feet to an angle point in said northerly line; thence South 01 degree 09 minutes 21 seconds West, along said northerly line, a distance of 22.00 feet to an angle point in said northerly line and the point of beginning; thence South 88 degrees 50 minutes 39 seconds East, along the northerly line of said Outlot B, a distance of 100.00 feet to an angle point in said northerly line; thence South 01 degree 09 minutes 21 seconds West a distance of 30.00 feet; thence North 88 degrees 50 minutes 39 seconds West a distance of 100.00 feet to the intersection with a line bearing South 01 degree 09 minutes 21 seconds West from the point of beginning; thence North 01 degree 09 minutes 21 seconds East a distance of 30.00 feet to the point of beginning.

WHEREAS, a Public Hearing notice was published by The Press on September 19, 2024 and mailed to property owners within 500' of the subject property. A Public Hearing was held by the City of Dayton Planning Commission on October 3, 2024; and,

COMPREHENSIVE PLAN AMENDMENT

WHEREAS, the Applicant requests the 2040 Comprehensive Plan, Figure 4: Staging Plan be amended such that the applicable properties (Dayton Creek Addition) change from “2020” to “Current”; and,

WHEREAS, City Code 1002.14 establishes a Growth Management Policy and criteria to open the next sewer stage (from “2020” to “Current”). The City Council finds that essential resources, facilities and services are available to serve the area southwest of I-94, including Dayton Creek Addition.

NOW, THEREFORE BE IT RESOLVED, the City Council authorizes Staff to solicit adjacent governmental units and affected school districts for their review, and to submit the Comprehensive Plan (2040 Staging Plan) to the Metropolitan Council for review.

PRELIMINARY PLAT

WHEREAS, in consideration of the application, the Staff Report, public testimony, and consistent with City Code 1002.05, Subd 1(2)(f)(4) (Planning Commission Action), the Planning Commission recommended APPROVAL with the following **Findings**;

- (a) That the proposed subdivision is **NOT** in conflict with the City's Comprehensive Plan, Zoning Ordinance, Capital Improvements Program, or other policy or regulation.
- (b) That the proposed subdivision is **NOT** in conflict with the purpose and intent of this chapter.
- (c) That the physical characteristics of the site, including but not limited to topography, vegetation, susceptibility to erosion, and siltation, susceptibility to flooding, water storage, and retention, are such that the site **IS** suitable for the type of development or use contemplated.
- (d) That the site **IS** physically suitable for the intensity or type of development or use contemplated.
- (e) That the design of the subdivision or the proposed improvements are **NOT** likely to cause substantial and irreversible environmental damage.
- (f) That the design of the subdivision or the type of improvements will **NOT** be detrimental to the health, safety or general welfare of the public.
- (g) That the design of the subdivision or the type of improvement will **NOT** conflict with easements on record or with easements established by judgment of a court.
- (h) That the subdivision is **NOT** premature as determined by the standards of Subsection 1002.03 of this section.

WHEREAS, the City Council considered the applications at its October 22, 2024 meeting. In consideration of the application, Staff Report, Public Testimony, and Planning Commission recommendation, the City Council **APPROVES** the Preliminary Plat with the following conditions:

1. The Preliminary Plat, dated September 25, 2024 shall be revised to comply with the City Engineer's letter, dated September 4, 2024 to the satisfaction of the City Engineer, prior to Final Plat approval.
2. The Applicant shall submit the Final Plat within one year of Preliminary Plat Approval, or this Approval shall be void.

Adopted this 22nd day of October, 2024, by the City of Dayton.

Mayor, Dennis Fisher

ATTEST

City Clerk, Amy Benting

Motion by Councilmember _____, Second by Councilmember _____.
The Motion passes.



Memo

| | | | |
|----------|---|-------|--|
| To: | Jon Sevald, Planning | From: | Jason Quisberg, Stantec Nick Findley, Stantec Josh Accola, Stantec |
| Project: | Dayton Creek Addition Prelim/Final Plat | Date: | September 4, 2024 |

Exhibits:

This Memorandum is based on a review of the following documents:

1. Dayton Creek Addition – Schany Group, LLC Plans, dated 08/22/2024 by WSB, 33 sheets
2. Dayton Creek Addition Preliminary Plat, dated 03/20/2024 by WSB, 3 sheets
3. Dayton Creek Addition Final Plat, undated by WSB, 3 sheets
4. Stormwater Management Plan – Rush Schany Phase 1, dated 07/19/2024 by WSB, 339 sheets
5. Dayton Creek Addition Autoturn Exhibit – Schany Group, LLC, dated 07/11/2024 by WSB, 1 sheet

Comments:

General

1. A preliminary/final plat submittal has been received by the City of Dayton for the Dayton Creek Addition (Schany property). The properties to be developed are located directly north and directly south of Dayton Parkway, just east of Brockton Lane N.
2. The plans provided for the preliminary/final plat submittal provide information for only the streets and utilities serving the properties eventually to be subdivided, not the properties themselves. It is our understanding that future plans would need to go through the platting process for each individual future property and a full review of the plans for those individual sites would be completed at that time.
3. Consistent with the review process, a comment response letter shall be provided in response to the following comments provided in this Memorandum in which the applicant provides a written response to each item.
4. In addition to engineering related comments per these plans, the proposed plans are subject to addition planning, zoning, land-use, and other applicable codes of the City of Dayton.
 - What are these comments. Why don't we have these comments to address prior to the Planning Commission and City Council meetings?
5. Any underlying easements no longer necessary must be vacated.
 - There are no underlying easements that are no longer necessary on the Schany parcel of property

6. Outlots shall be covered by drainage and utility easements. This was discussed with the applicant (9/12/2023 council meeting), applicant suggested they were going to review and develop a possible solution. As this is a city code requirement a variance would be required for easements not to be obtained over the outlots.
 - o Please show us where in the city code an outlot is required to have a drainage and utility easement over any outlot. In previous meetings and conversations with the developer and staff, it is the developers' understanding that the staff has agreed that the drainage and utility easements can be shown on the Preliminary Plat and not on the final plat.
7. Compliance with City Standard Detail Plates, along with other ordinances, rules, policies and practices will be required. Update the standard details to be the most recent set. Coordinate with engineer.
 - o Resolved - Our plans have details dated Jan 2023. We will coordinate with the engineer to get the most recent set of details with Construction Documents.
8. The current set appears to be missing details relating to storm sewer manholes, flared end sections, draintile and other items. Provide all applicable Dayton Standard details.
 - o Resolved - Storm sewer details will be added with Construction Documents.
9. There are multiple instances of callouts that appear to be no longer needed, covering other items, or not pointing to the correct items. Revise.
 - o Resolved - The call outs that are not needed will be removed with Construction Documents.
10. The roadway alignment was adjusted for the east west road to minimize wetland disturbance until the south roadway is extended. The roadway will have to be reconstructed to the original alignment when the south roadway is extended. Developer is to provide an escrow for the completion of this work.
 - o Developer understands that a surety, bond or letter of credit will be provided for this work scope.

Plat

11. Right of way to be platted for a roadway and utility extensions to the north property line. After council discussed with the applicant (9/12/2023 council meeting), council suggested that this determining the right of way with the development of Outlot B would be acceptable.
 - o No action needed.
12. The boundary lines for the outlots appear to follow the delineated wetland boundaries. The outlots should be adjusted to include also the wetland buffer areas. After council discussed with the applicant (9/12/2023 council meeting), council suggested that wetlands buffers could remain within the outlots listed as buildable.
 - o No action needed.
13. Recommendation from engineering is an 80' right of way with 40' roadway for the east west road. After council discussed with applicant (9/12/2023 council meeting), council suggested a 60' right of way could be considered for the east west road. Currently the roadway is proposed as 50' right of way in the plat documents and 46' in the typical section. Revise ROW to be 60' with a 36' wide roadway (face to face) to provide for two

lanes of traffic with room for emergency vehicles or disabled vehicles without severely interrupting the flow of traffic.

- o City code states that a minor or marginal street is a street primarily used for access to abutting properties. The south private street (east-west road) will be used for access to future abutting properties (commercial developments in Outlot F). The street will not provide access or connections to any other roads, and by definition meets the minor or marginal street classification. The right-of-way width required for a minor or marginal is 50 feet per city code. The only classification types that would potentially require a 60' right-of-way width are local streets or neighborhood collectors. By definition, both local streets and neighborhood collector streets connect neighborhoods and provide access to local and collector streets (neighborhood collectors also provide access to minor arterials). The south private street does not fit the definition of a local or neighborhood collector road and would therefore not require the requested 60' right-of-way.

City code:

Street, Community Collector

A street that connects communities within and between subregions; provides mobility between communities; and provides access to other streets, as found on the Comprehensive Plan.

Street, Local

A street that connects blocks within neighborhoods, provides mobility within neighborhoods, and provides access to neighborhood collector streets and other local streets, as found on the Comprehensive Plan.

Street, Minor Arterial

A street that connects adjacent subregions and activity centers within subregions; provides mobility within and between 2 subregions; and provides access to interstate freeways, major arterial streets, other minor arterial streets, and collector streets, as found on the Comprehensive Plan.

Street, Minor or Marginal

A street that is used primarily for access to abutting properties.

Street, Neighborhood Collector

A street that connects neighborhoods within and between subregions; provides mobility between neighborhoods; provides access to minor arterials, other community collector streets, other neighborhood collector streets and local streets, as found on the Comprehensive Plan.

(3) Width of right-of way.

a. For all public ways hereafter dedicated and accepted, the minimum right-of-way widths for streets shall be as shown in the City Comprehensive Plan, and where not shown therein, the minimum right-of-way width for streets, alleys, or pedestrian ways included in any subdivision shall not be less than the minimum dimensions for each classification as follows:

| | |
|--------------------------|-----------------|
| Minor arterial | 100 to 150 feet |
| Community collector | 80 to 100 feet |
| Neighborhood collector | 60 to 80 feet |
| Local street | 50 to 60 feet |
| Minor or marginal street | 50 feet |
| Pedestrian way | 10 to 20 feet |

14. Provide 20 ft of easement centered over proposed watermain and storm sewer in any location where it is not provided by ROW. This can be completed using a combination of ROW and easement.
 - o Resolved - an easement will be provided after city review for water outside of right-of-way.
15. Provide a 16' wide horizontal and vertical alignment showing and emergency access route with applicable easement covering the alignment from the end of the E-W Street to Brockton Lane. Responsibility of construction to be discussed further.
 - o This is a new comment that we have not heard about previously. The City resisted our attempts to get a right in/right out from Brockton to the site when we brought it up and were told we could not do it. It is the applicant's position

that this should be discussed with the development of Outlot H, we are not developing Outlot H at this time.

Erosion Control/SWPPP

16. Obtaining and complying with a MPCA Construction Stormwater Permit (NPDES permit) will be required for this project.
 - o Resolved - this is a construction permit and cannot be applied for until Construction Documents are complete and before construction.
17. A City of Dayton Land Disturbance Permit will be required.
 - o Resolved - this is a construction permit and cannot be applied for until Construction Documents are complete and before construction.
18. Construction entrances should be ~50 feet long oriented in the direction of travel.
 - o Resolved - this will be revised on the Construction Documents.
19. Disturbances are over an acre which triggers the requirement of a SWPPP. Please provide with next submittal.
 - o Resolved - a SWPPP will be provided with Construction Documents.
20. Grading is shown outside of perimeter control, provide additional information detailing disturbed areas do not flow into neighboring waterbodies/wetlands.
 - o Resolved - grading and silt fence will be revised to ensure no wetland fill beyond what has been permitted will occur.
21. Silt Fence must remain outside of wetland areas unless disturbance is permitted. Double silt fence to be used along wetlands per MPCA Construction Stormwater permit.
 - o Resolved - grading and silt fence will be revised to ensure no wetland fill beyond what has been permitted will occur. Double silt fence adjacent to the wetlands will be added to the Construction Documents.
22. J-hooking silt fence will be required where silt fence runs perpendicular to contours. Show on plans.
 - o Resolved - this will be shown on the Construction Documents.
23. Silt fence should not be used in channels. Alternative perimeter control that allows flows to overtop should be provided (e.g. biologs, rock bags, hay bales, etc.)
 - o Resolved - this will be shown on the Construction Documents.
24. Sod and seeding are shown in roadway/pavement for restoration/erosion control. Correct plans to show erosion control in appropriate places.
 - o Resolved - this will be shown on the Construction Documents.

Grading/Stormwater

25. Because of the large number of issues noted from a drainage/stormwater standpoint, there may be additional stormwater comments upon future reviews.
 - o You have given us your exhaustive list of comments. After 7 weeks of review time, we would expect that the City has completed their comments. We met and came to terms of how these items will be addressed and we will address these comments with our upcoming SWMP and Construction Documents. Once these items are addressed we would expect that no additional comments will be forthcoming.

26. Elm Creek Watershed review and approval will be required.

- Elm Creek Watershed requires City approval prior to submitting it for watershed review. We will be submitting to the watershed as soon as we receive City approval.

27. Phased developments such as this may want to provide a full build out conditions plan and modeling to ensure that the overall plan will be compliant. Any variance to stormwater and erosion control will not be approved if due to lack of foresight on phasing/planning.

- It is typical for commercial projects to provide stormwater treatment on each individual lots. Especially in this instance since the lots are surrounded by wetlands. It is not a code requirement for the stormwater design for future commercial development to be provided. The impervious area for these future developments is not known at this time and therefore we are unable to calculate stormwater treatment for these.
- In order to take advantage of extra capacity for the south parcel we will provide a stub between the parcel southwest of Dayton Parkway and the south basin. The stub will allow a future developer to utilize any excess capacity of the BMP.

28. Ponds/BMPs required for future development areas will be privately owned and maintained. Stormwater maintenance agreement(s) will be required at that time.

- Resolved - when each individual parcel develops, the city and watershed will have review authority and will be able to require stormwater maintenance agreements for future stormwater treatment basins.

29. The Stormwater Management Plan notes in the 3.0 Proposed Condition section, "This phase will add proposed public roadways and sidewalks north and south of Dayton Pkwy, proposed commercial lots, and stormwater BMPs. The proposed phase also consists of two filtration basins adjacent to the north property line to control the stormwater runoff entering the site created by the large existing catchment area north of the property." However, no commercial lots can be proposed because no stormwater management has been provided. They are only outlots until they have stormwater management. Additionally, no grading or modeling is shown for the 2 "filtration basins on the north property line." These will not be permitted unless they have been modeled and approved.

Resolved - the following will be reflected in our upcoming SWMP and Construction Documents.

- The plan removed any reference to future development. All future development must provide their own BMPs.
- Only the BMPs proposed with this project are shown on the plans and in the model. Any reference to additional BMPs has been removed from the SWMP.
- This was corrected in the report to state "2 filtration basins on the north and south property line".
- A note will be added to the plans and SWMP about a stub and excess BMP capacity for a future developer to use to supplement their required

stormwater management.

30. The Stormwater Management Plan notes in the 3.0 Proposed Condition section, *"This resulted in the northern portion of this road draining north to the proposed northern filtration basin and the southern portion of this road draining south to the existing Great Blue Heron Pond, Pond 2P, southwest of Dayton Pkwy. This is due to drainage patterns and the Elm Creek Management Commission's permit 2019-025. As a result, the southern portion of this proposed road was merged with the existing Subcatchment EX 4AS to form Subcatchment PR 4AS and routed to the existing Great Blue Heron Pond, Pond 2P. Using the provided existing As-built with provided grading and storm sewer plans, the Great Blue Heron Pond was modeled in both Existing and Proposed Conditions. The results of this prove that the Great Blue Heron Pond will have capacity to hold and treat drainage, and meet rate control and water quality volume requirements from the southern half of the proposed half of the northern roadway."* This does not "prove" anything. We received the most current SWMP from SRF for the Dayton Interchange Project and it does not account for the southern portion of the northern roadway draining to Great Blue Heron Pond. Please provide documentation to justify drainage to Great Blue Heron Pond or adjust design. Additionally, drainage areas do not match. If this pond is to be used, full accounting of how much additional capacity is available, will be required to evaluate if peak flow and water quality control can be met. Water quality modeling must be provided for the areas draining to Great Blue Heron BMP.

Resolved - the following will be reflected in our upcoming SWMP and Construction Documents.

- A complete analysis considering existing and proposed conditions of the Great Blue Heron pond will be provided in the SWMP.
- The MIDS model with the GBH pond both modeled in existing and proposed conditions will be provided.

31. Update drainage boundaries to be in accordance with the Dayton Interchange project for existing and per proposed contours for proposed conditions. Missing offsite drainage into 2S and 7AS from west side of Brockton Lane roadway.

Resolved - the following will be reflected in our upcoming SWMP and Construction Documents.

- Drainage areas and model have been updated accordingly to be in accordance with the Dayton Interchange project.
- Model has been modified to include all offsite drainage.
- Drainage areas have been adjusted to reflect the proposed grading.
- New figures were provided that show the updated drainage boundaries.

32. Pretreatment is required prior to filtration basins.

Resolved - the following will be reflected in our upcoming SWMP and Construction

Documents.

- Pretreatment will be provided via grass swale and sump structures. These measures will be added to the plans and drainage report.
33. EOF should only be utilized in excess of the 100-year event. Dayton requires one (1) foot of freeboard from the 100-year HWL to the EOF Spillway invert. Top of berm should be 1 foot above EOF. Both filtration basins are in conflict with this rule. Call out HWL, EOF, Top of Berm, Filter Bottom, etc. on plans. EOF freeboard during "resiliency analysis" is still required.
- Resolved - the following will be reflected in our upcoming SWMP and Construction Documents.**
- The pond designs will be updated to provide 1 foot of freeboard from the 100-year HWL to the EOF. The top of berm is 1 foot above the EOF.
 - HWL, EOF, top of berm, filter bottom, and outlet elevations will be called out on the grading plans.
 - A resiliency analysis was previously provided and will be redone with the proposed model. A separate section discussing the resiliency analysis is already in the report.
34. Filtration basin details including outlet structures must be provided within plan set. Add orifice and capping system to allow for drawdown before winter and for maintenance. All bends or connections shall require clean outs for the underdrain.
- **Resolved - filtration basin and outlet control structure details will be added to the plans and reflected in Construction Documents.**
35. A significant amount of flow occurs between the I-94 ROW and the ACOE wetland at the NE corner of Brockton Lane and Dayton Parkway. It is critical that the stormwater management plan for the north property appropriately accounts for this flow, and proposed improvements adequately convey said flows. See Dayton Interchange SWMP for offsite areas draining to 2S and 7AS which are not accounted for. Update modeling to account for all offsite flow.
- **Resolved - the existing and proposed modeling has been adjusted to account for all of the offsite flow, consistent with the Dayton Interchange analysis and will be reflected in our upcoming SWMP.**
36. Similarly, to the previous comment, there is significant flow between the discharge from the ACOE (south of the parkway) and the wetland abutting Rush Creek. It is critical that the stormwater management plan for the south property appropriately accounts for this flow, and proposed improvements adequately convey said flows.
- **Resolved - the modeling has been adjusted to account for all of the offsite flow, consistent with the Dayton Interchange analysis and will be reflected in our upcoming SWMP.**
37. Provide electronic models and spreadsheets for all calculations every time there is a

submittal.

- Resolved - all models that have been requested up to this point have been provided. The electronic models of the existing and proposed HydroCAD models, the proposed BMP analysis HydroCAD model, and the spreadsheets for the storm sewer design and MIDS models will be provided with the upcoming SWMP.
38. SWMP notes that there are no impaired waters within 1 mile radius, but Rush Creek is impaired and directly adjacent to the project.
- Resolved - the upcoming SWMP will reflect this.
39. HydroCAD model shows 100-year water surface elevations increasing between existing and proposed conditions for Great Blue Heron Pond.
- Resolved - see the Great Blue Heron Pond analysis in the SWMP for a complete overview of the basin. HWLs increase because additional water is routed to the basin. It will be ensured that the 100-year high water level is at least 1-foot below the required freeboard to the EOF.
40. There are significant issues with how existing pipes, structures, and BMPs are modeled compared to record plans. See sheets 246-277 of record plans for drainage plans. See 274 of 593 of plans for outlet structures.
- Resolved - all pipes, structures, and BMPs have been revised to match the record plans and will be reflected on the upcoming SWMP. It should be noted that we didn't receive the record plans with the first round of engineering comments. We received them on September 6th, 2024.
41. 2P or Great Blue Heron Pond is not modeled correctly. Normal or starting water level must be set in HydroCAD. Wet pond is modeled as starting empty. Additionally, outlet structure is not being modeled correctly.
- Resolved - A starting elevation has been added to Great Blue Heron pond at the NWL. The outlet structure has been updated to match the record plans and will be reflected in the Construction Documents.
42. 3P or Great Blue Heron Filtration is not modeled correctly. Bottom is 913.0 per record plans. HydroCAD has it starting at 911.0. Outlet modeled here should actually be applied at 2P.
- Resolved - the model has been modified to match the plans and will be reflected in our upcoming SWMP.
43. 9P or Wood Duck Pond/Filtration system is not modeled per plans. A weir in/at the structure at 908.5 is shown on plans but is not modeled. Starting water surface elevation (908.5) is not modeled. Pond is modeled as starting empty.
- Resolved - A starting elevation has been added at the NWL. The outlet structure has been updated to match the record plans and reflected in the upcoming SWMP and Construction Documents.

44. 2S wetland is a public water wetland and should be modeled with a CN of 98.

- Resolved - model has been updated according to the Dayton Interchange model and will be reflected in our upcoming SWMP. It is important to note that we just received the model from the city on September 6, 2024.

45. 2S wetland storage or 6P outlet should be modeled as 2-36" and 1-42" pipe not a 15" to a 24" as shown in HydroCAD.

- Resolved - model has been updated according to the Dayton Interchange model and will be reflected in our upcoming SWMP.

46. Maximum predevelopment CNs for HSG C and HSG D are 72 and 79, respectively. Soils that are dual classification default to the 2nd HSG unless the soils have been drained (e.g. tile drainage installed). Additionally, all filtration areas are considered impervious (CN 98) to avoid double counting.

- Resolved - model has been updated according to the Dayton Interchange model and will be reflected in our upcoming SWMP.

47. EX/PR 2S wetland drains to EX/PR 5S not to existing ditch to Wood Duck BMP as shown in HydroCAD routing.

- Resolved - model has been updated according to the Dayton Interchange model and will be reflected in our upcoming SWMP.

48. Culvert under north roadway is not modeled. Culverts should be modeled as outlet of 1S/P not separately off to the side in HydroCAD. Major culvert crossings require a minimum 50-year frequency per MNDOT requirements. Please model the 50-year design event.

Resolved - the following will be reflected in our upcoming SWMP.

- Culvert has been added to the model and is no longer modeled off to the site.
- The model now includes 50-year design results. An analysis of the 50-year storm will be added to the SWMP.

49. Culverts between 7AS (11P) and 7BS (8P) are not modeled.

- Resolved - these culverts have been added to the model and will be reflected in the upcoming SWMP.

50. Proposed grading outside of roadway areas is shown on drainage figures and does not match plans. Please revise so only/all grading shown on plans is shown on drainage figures.

- Resolved - the proposed figures have been updated and will be reflected in the upcoming SWMP.

51. Summary Table 1.1.1 notes that EX 1S and PR 1S are identical, but drainage boundary changes between existing and proposed. Please rectify any table discrepancies like this.

- Resolved - drainage areas have been revised and will be reflected in the upcoming SWMP.

52. Summary Table 1.1.2 shows HydroCAD node discharge rate, but rates should be compared at outfalls not individual nodes. Peak rate control must be met at each discharge location not just final discharge location of Rush Creek.

- Resolved - per discussions with the City's engineers, the rate control analysis will be updated to analyze discharges at Wood Duck pond, 8S and 6S and reflected in the upcoming SWMP.

53. Table 4.0.1 notes that the provided treatment volume is 76,047 cf. This is not correct. The treatment volume is only the volume between the bottom of the filtration area (913.0) and the constructed overflow (914.5). Storage provided is roughly 35,600 cf. Dayton Interchange SWMP shows a drainage area of 5.4 acres to Great Blue Heron Pond/Filter.

- Resolved - the treatment volumes have been corrected and will be reflected in the upcoming SWMP.

54. Great Blue Heron Filtration required an impermeable liner due to seasonal high groundwater. Provide an impermeable liner for filtration basins.

- Resolved - an impermeable liner has been added to the details. This has also been added to the drainage report and will be reflected in the upcoming SWMP and Construction Documents.

55. Call out and provide details for pedestrian ramps including spot elevations ensuring they meet ADA standards.

- Resolved - this will be provided in the upcoming Construction Documents.

56. There are multiple instances of contours not tying in, contours having "gaps" and other discrepancies. Below some of these locations are listed, revise all instances throughout the site.

- Southeast corner of Dayton Parkway and proposed north-south road does not tie into existing contours.
- Northwest corner of Dayton Parkway and proposed north-south road does not tie into existing contours.
- The 917 contour along the south side of the east-west road near station 33+25 ties into existing contours that would make grading 2:1 with the adjacent 918 contour.

- The 916 contour along the south side of the east-west road near station 33+50 ends abruptly without tying in.
- The 924 contour along the west side of the north road near station 15+25 ends abruptly without tying in.

Resolved - the grading will be updated in the upcoming Construction Documents.

57. Where feasible provide fill along the future road alignment of the east west road to allow for an easier connection at a future date.

- **Resolved - this project includes grading for the roads which will tie into existing contours.**

58. Provide EOF spot elevations for low point catchbasins and basins.

- **Resolved - this will be provided and shown in the upcoming drainage and Construction Plans.**

Wetlands

59. Elm Creek will require wetland buffers. Outlots currently don't account for buffers, it should be noted this may lead to potential enforcement issues in the future.

Establishment of wetland buffers are required in locations where disturbances are within 50' of wetlands.

- **Resolved - wetland buffers adjacent to the disturbed/grading will be added with the Construction Documents.**

60. It should be noted that the large wetland complex north of the parkway is known to be under ACOE jurisdiction. This project has been coordinated with the Army Corps and it is believed that no permitting will be required. It appears there is areas where grading and other items encroach wetlands that have not appeared in any mitigations plans to date. Wetland permitting, delineations and impact/mitigation plan are to be confirmed with applicable entities.

- **Resolved - grading will be modified to occur outside of wetlands not intended to be impacted.**

61. Silt fence located within wetlands, applicable wetland permitting will be required for the disturbance.

- **Resolved - silt fence and grading impacts have been adjusted slightly as necessary to remove from the wetland. This will be shown in the upcoming Construction Documents.**

Floodplain

62. Compensatory storage will need to be provided below the BFE and above the ordinary high water level (OHWL) for any floodplain fill and no disturbance within floodway is permitted. Any fill material placed within the designated floodplain must be mitigated by providing compensatory storage. Appropriate FIRM mapping revisions or amendments will be required. OHWL determined in consultation with DNR is 908.0

- **Resolved - compensatory storage has been provided resulting in a net cut below the floodplain elevation. This project proposes 8775 CY of cut and 8669 CY of fill below the floodplain elevation of 913.33 feet. The resultant is 106 CY of net cut.**

63. A floodway and floodfringe has been defined for North Fork of Rush Creek. Use this to

September 4, 2024

Dayton Creek Addition

Jon Sevald

determine where building/grading can occur within the floodplain and what rules apply in each zone.

- Resolved - according to the City's study that was completed in this area, there is not a floodway over the project area. See the SWMP. All work is proposed within the flood fringe.

64. Elm Creek HUC-8 Flood Study is in development which may change the flood maps and regulatory model used in this area. We recommend the developer consider this study in development plans to avoid surprises of being mapped into a floodplain due to a change in the regulatory model and SFHAs.

- Resolved - our calculations use the current information available. The HUC-8 study is still in development and has not yet been approved. Upon request, the DNR did not provide the model for our use.

65. Rush Creek is the primary flooding source for the FEMA SFHA Zone A floodplain.

- Resolved - correct, Rush Creek is the primary source of flooding and this project is within a Zone A FEMA floodplain.

66. The water surface elevations (WSE) reported in the Memo match modeling, but there are discrepancies between the report and model geometries - mainly channel geometry and ineffective flow areas (detailed below).

- Resolved - discrepancies have been revised and will be included in the upcoming SWMP.

67. The 'BackWater' reach should be separated from Rush Creek and used to create a new model / plan. This will allow for more detailed modeling of Rush Creek as the entirety of the proposed project should be included in the geometry. The cross sections will be allowed to cross as they are part of a separate model / plan.

- Resolved - the City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.

68. Comments related to individual (separated) Rush Creek and BackWater geometries are detailed below.

- Comment, no response required.

69. Figure E1 will also need to be updated to show Rush Creek and 'BackWater' individually.

- Resolved - the figure will be updated accordingly.

Rush Creek

70. Combine the two (2) Rush Creek reaches (Rush Creek, Rush Creek South)

- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.

71. Add cross sections (XS) along Rush Creek through the entirety of the proposed project (3-4 total XSs through the project seems sufficient)
- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.
72. Check XS locations on Figure E1 – the Brockton Lane N bridge is located between XS 4582 and 4550 upstream and downstream, respectively.
- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.

BackWater

73. Change downstream reach boundary condition to 'normal depth' based on channel slope.
- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.
74. Cut additional XSs downstream to confluence with Rush Creek
- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.
75. Define channel for each XS (e.g. Bridge 9.5 – channel at 908.74' and just downstream XS 9 – channel at 911.34'). If no survey available, may need to be manually lowered to account for LiDAR.
- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.
76. Check bank stations after channel defined.
- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment

response 62, the project remains out of the floodway and compensatory storage has been provided.

77. Adjust overbank manning's n values to match land use (e.g. emergent wetland – 0.075-0.15).

- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.

| NLCD Value | NLCD Classification | Manning's n (Min) | Manning's n (Normal) | Manning's n (Max) | Source |
|------------|-----------------------------|-------------------|----------------------|-------------------|----------------------|
| 11 | Open Water | 0.025 | 0.030 | 0.033 | Chow 1959 |
| 21 | Developed, Open Space | 0.010 | 0.013 | 0.016 | Calenda, et al. 2005 |
| 22 | Developed, Low Intensity | 0.038 | 0.050 | 0.063 | Calenda, et al. 2005 |
| 23 | Developed, Medium Intensity | 0.056 | 0.075 | 0.094 | Calenda, et al. 2005 |
| 24 | Developed, High Intensity | 0.075 | 0.100 | 0.125 | Calenda, et al. 2005 |
| 31 | Barren Land | 0.025 | 0.030 | 0.035 | Chow 1959 |
| 41 | Deciduous Forest | 0.100 | 0.120 | 0.160 | Chow 1959 |
| 42 | Evergreen Forest | 0.100 | 0.120 | 0.160 | Chow 1959 |
| 43 | Mixed Forest | 0.100 | 0.120 | 0.160 | Chow 1959 |
| 52 | Scrub/Shrub | 0.035 | 0.050 | 0.070 | Chow 1959 |
| 71 | Grassland Herbaceous | 0.025 | 0.030 | 0.035 | Chow 1959 |
| 81 | Pasture/Hay | 0.030 | 0.040 | 0.050 | Chow 1959 |
| 82 | Cultivated Crops | 0.025 | 0.035 | 0.045 | Chow 1959 |
| 90 | Woody Wetlands | 0.080 | 0.100 | 0.120 | Chow 1959 |
| 95 | Emergent Herbaceous Wetland | 0.075 | 0.100 | 0.150 | Chow 1959 |

78. Add ineffective flow areas upstream, downstream, and at bridges (9.5, 13.5) using RAS four (4) XS approach.

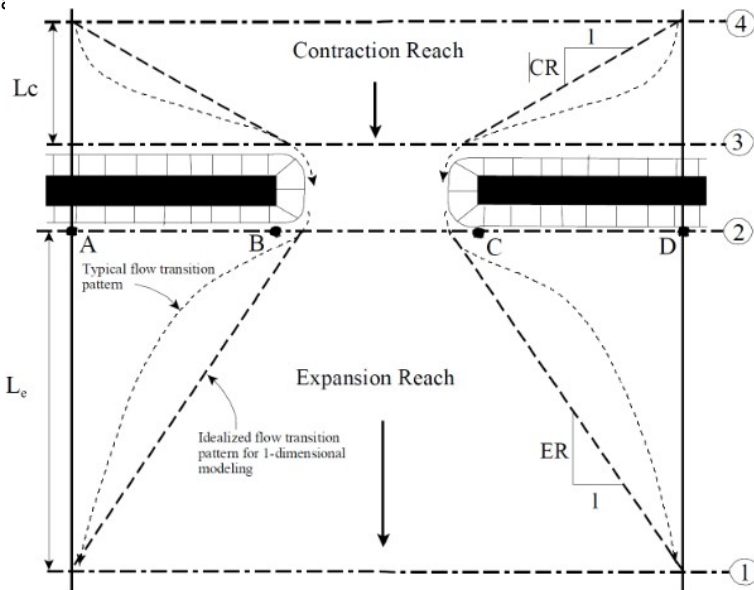
- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.

September 4, 2024

Dayton Creek Addition

Jon Sevald

P:



79. Set expansion/contraction coefficients to 0.3/0.5 for two (2) XS upstream and one (1) XS downstream.

- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.

80. Reported 'BackWater' 100-year WSE should be based on the relationship below:

- 'BackWater' WSE < Rush Creek WSE = Rush Creek WSE
- 'BackWater' WSE > Rush Creek WSE = 'BackWater' WSE
- Resolved - The City's engineers stated that as long as the project remains out of the floodway and compensatory storage is provided within the flood fringe, there is no additional modeling that would be required. As addressed in comment response 62, the project remains out of the floodway and compensatory storage has been provided.

Water Quality

81. PDF of area inputs in SWMP doesn't match models provided. SWMP must match modeling.

- Resolved - the SWMP areas and water quality modeling areas will be updated to match one another.

82. Existing modeling doesn't model impermeable liner of Great Blue Heron filter.

- Resolved - An impermeable liner has been added to the Great Blue Heron filter in the model and will be included with the updated SWMP.

83. Bioretention systems should use Media Mix C, not A, as noted in the water quality inputs.

- Resolved - the model will be updated to use Media Mix C for all of the

September 4, 2024

Dayton Creek Addition

Jon Sevald

bioretention system systems and will be included in the upcoming SWMP.

84. Water quality modeling notes iron enhanced layer of which there is no evidence of in the plans. Details of bioretention system must be provided.

- Resolved – cross section details of the bioretention system will be added to the plans with the upcoming Construction Documents.

85. Underlying soil infiltration rate in water quality modeling is too high considering HSG C and D. Regardless, impermeable liners should be provided for filtration BMPs as was required for Great Blue Heron Filter.

- Resolved – an impermeable liner has been added to all proposed filtration BMPs and will be shown in the upcoming SWMP.

86. Overflow depth is 3 feet not 2 feet as shown in water quality inputs.

- Resolved – the overflow depth in the MIDS model will be updated to match the proposed basin design and included in the upcoming SWMP.

87. Bypass needs to be considered in water quality calculations. See MN Stormwater Manual for guidance.

- Resolved – From the MN stormwater manual, the bypass is 8%. The bypass has been added to the water quality model for all bioretention basins and will be included in the upcoming SWMP.

88. MIDS modeling shows all disturbed areas being treated which is not correct. Only areas routed to BMPs can be modeled as routed to BMPs.

- Resolved – the model will be updated to only route areas to BMPs that are routed to BMPs via overland flow or storm sewer. This will be included in the upcoming SWMP.

Storm Sewer

89. Correct figure of subwatersheds to each catchbasin/inlet. Drainage boundaries do not reflect grading shown. This includes drainage to existing inlets along Dayton Parkway if design remains the same. For example, southern road cul-du-sac is not being routed to inlets (draining west) per plan grading but is shown as all draining to inlets on storm

sewer drainage map. Additionally, northern road cul-du-sac does not show offsite area that is draining to inlets. Drainage boundaries must reflect grading. Fix all instances.

- Resolved – the storm sewer drainage area map will be accurately updated to reflect the proposed grading and storm sewer. The offsite drainage will be adequately accounted for in the calculations. This will be reflected in the upcoming SWMP.
90. 5102 and 5101 drainage areas are modeled as identical but there is more impervious in 5102 than 5101. Please correct this and other discrepancies within the spreadsheets. Provide spreadsheets for easier checking.
- Resolved – the impervious areas discrepancies will be updated in the calculations and the spreadsheets will be provided with the upcoming SWMP.
91. Why are 5005 and 5001 stool type inlets if they are in the curb line?
- Resolved – these will be revised to regular catch basin structure.
92. Catchbasin spacing or spread cannot be evaluated. Numerous structure IDs/names given that do not appear to be related to this project. Label drainage areas on plans to match calculations. Evaluation of spread to existing inlets must be provided if design stays the same.
- Resolved – the updated drainage area maps, plans and calculations with matching IDs will be provided and the spread will be reviewed at each inlet. This will be reflected in the upcoming SWMP.
93. Provide profiles for all proposed storm sewer.
- Resolved - this will be shown on the upcoming Construction Documents.
94. Provide VB style castings at low points.
- Resolved – VB style castings will be provided and will be identified in our upcoming Construction Documents.

Watermain/Water Supply

95. A MDH Water Extension permit will be required.
- Resolved - This will be applied for after City review is completed.
96. Water sourcing is being determined with the adjacent communities of Rogers and Maple Grove. There may be limitations to water availability in the area.
- We assume that this will be determined in time for this project to be constructed spring of 2025.
97. The plans indicate water supply for both parcels being accomplished via a connection to Rogers from an existing stub at the end of Blue Stem Ct in Rogers. The plans show directionally drilling a 18" carrier pipe with 12" watermain under Brockton Lane (a Hennepin County Road). The method for achieving this connection to the watermain stubbed from Blue Stem Ct must be approved by Hennepin County. The method the County approves for providing the water connection beneath Brockton Lane will also be utilized by the developer and allowed by the City for the connection to the south side of Dayton Parkway.
- Noted. We have reached out to the County to get feedback.
98. No bends greater than 45 degrees will be allowed.
- Resolved -

September 4, 2024

Dayton Creek Addition

Jon Sevald

99. Include hydrants and fittings in the profiles.

- o Resolved - this will be completed with Construction Documents.

100. Only hydrant leads are to be DIP, revise watermain services to be C-900.

- o Resolved - this will be completed with Construction Documents.

101. Include "Maintain a minimum of 18" separation" note to all locations using insulation, similar to the call out shown on sheet 21.

- o Resolved - this will be completed with Construction Documents.

102. The watermain is shown offsetting under the proposed box culvert. Due to the size of the box culvert the watermain is at a greater freezing risk than standard storm sewer crossings. Please provide 4' feet of separation and insulation between the bottom of box culvert and top of pipe.

- o Resolved - this will be completed with Construction Documents.

103. Watermain service to Outlot I is shown within the proposed roadway, revise to extend to the property line.

- o Resolved - Outlot I is a wetland and will not need water service.

104. Provide mid span hydrant along North Road.

- o Resolved - this will be completed with Construction Documents.

105. Provide gate valve at existing water connection.

- o Resolved - this will be completed with Construction Documents.

Wastewater Collection/Sanitary Sewer

106. A MPCA Sewer Extension permit will be required.

- o Resolved - this will be applied for once City review is completed.

107. Include the material type for sanitary sewer based on the depth requirements provided in detail Gen 12.

- o Resolved - this will be completed with Construction Documents.

108. Stub the sanitary sewer north of manhole 6103 to be 25' past the proposed watermain to ensure watermain is not undermined when a future connection is made.

- o Resolved - The water will be adjusted to maintain 25' from the end of the sanitary sewer.

109. Reduce sanitary sewer slopes to be the minimum laid out in the 10 State Standards.

- o Resolved - this will be completed with Construction Documents.

Transportation

110. A traffic signal will be necessary when the first property in the area develops. City council has initiated a project involving traffic signals at this location. It is expected that at least a portion of the funding will come from special assessments and/or developer participation in the area.

- o It is our understanding that all developers, landowners, and the City will be participating in the cost of the signal.

111. In areas where existing roadway is being widened, ensure all small utilities are relocated outside of the proposed widening.

- o This should not be a developer cost since the roadway should have been wide enough when built.

112. Contractor to provide a minimum of 48-hour notice before removals take place along

September 4, 2024

Dayton Creek Addition

Jon Sevald

Dayton Parkway. Appropriate traffic control will be required.

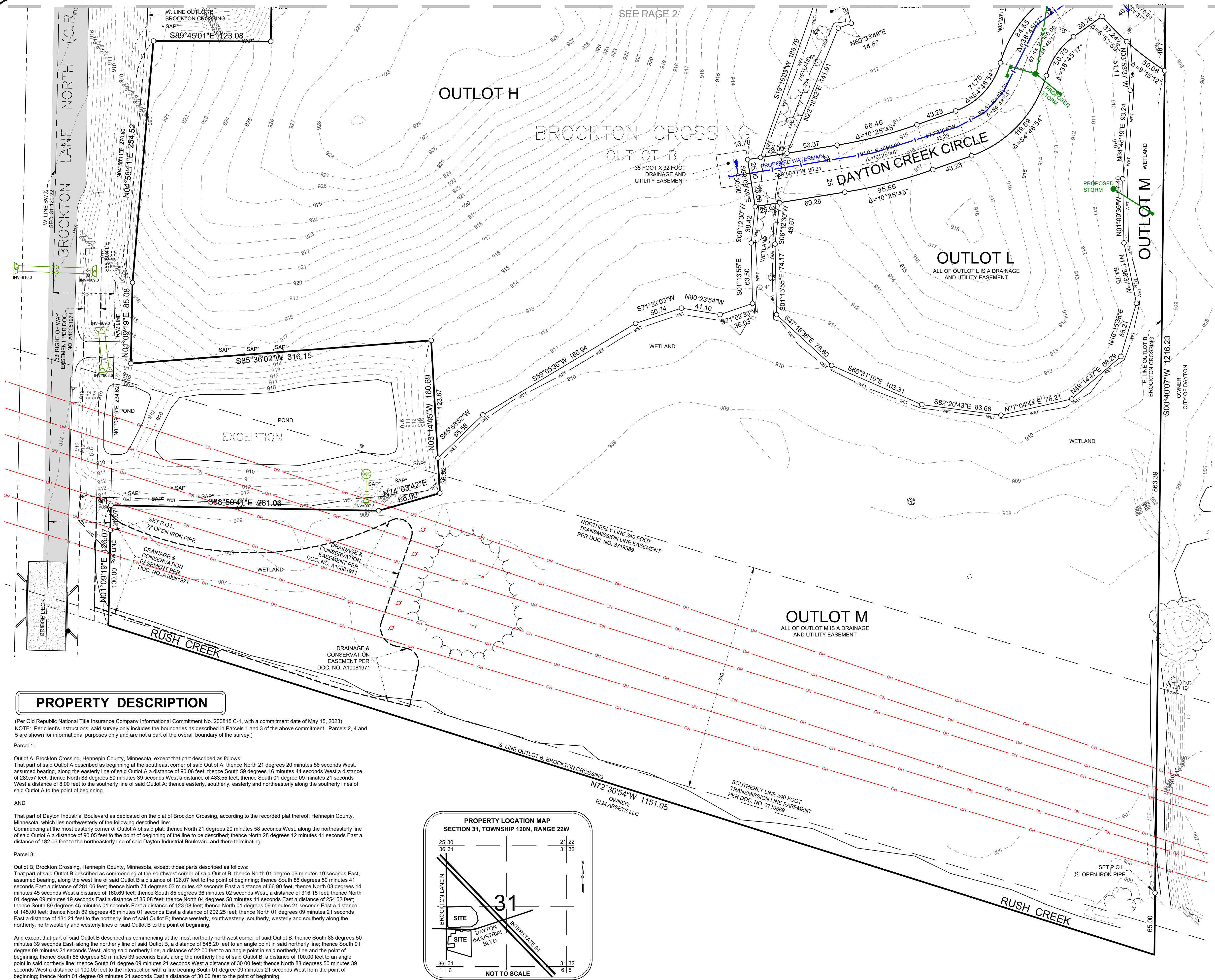
- **Noted.**
- 113. If boulevard is less than 4' provide a concrete maintenance strip in lieu of turf.
 - **Resolved - this will be completed with Construction Documents.**
- 114. Provide no parking signs along the proposed roadways.
 - **Resolved - this will be completed with Construction Documents.**
- 115. Provide road subdrain draintile per standard detail STO-13A.
 - **Resolved - this will be completed with Construction Documents.**
- 116. Update radius on the northwest corner of the intersection of the South Road and South E-W Road to be a minimum of 20'.
 - **Resolved - this will be completed with Construction Documents.**
- 117. Provide street signs at the intersection of South Road and South E-W Road.
 - **Resolved - this will be completed with Construction Documents.**
- 118. Provide a stop sign for the E-W road where it ties into the south road.
 - **Resolved - this will be completed with Construction Documents.**
- 119. Provide speed limit signs on all roadways.
 - **Resolved - this will be completed with Construction Documents.**
- 120. Grade breaks are not to be used in street design. Revise to ensure curves are used.
 - **The road has been designed with vertical curves. Please clarify where grade breaks are used in lieu of vertical curves.**
- 121. Provide taper lengths and curb radii where applicable.
 - **Resolved - this will be completed with Construction Documents.**

Other Comments

- 122. The City of Dayton owns the adjacent parcel to the east, south of the parkway. Encroachments, access, staging areas, etc. can be discussed as appropriate.
 - **Noted.**
- 123. It may be desirable to provide a looped trail along the north side of the street currently shown as private. The looped trail would connect to a future trail along Brockton, the trail along Dayton Parkway, and the trail along the south Public Road proposed with this preliminary plat. This should be discussed. A trail easement over the trail connection between the end of the cul-de-sac and the connection to the future Brockton trail would be required.
 - **Trails will be provided along the roads for this road project. This project does not include the development of the outlots. This can be discussed at the time of development for Outlot H.**
- 124. Provide street lighting along all proposed streets.
 - **Resolved - this will be completed with Construction Documents.**
- 125. Currently trails are proposed along the east and sidewalk along the west of the North and South Road. Revise to have trail along the west and sidewalk along the east of the North and South Road.
 - **We completed the design of the roads based on the City street section detail. This should have been commented on at the time of initial comments. We are not going to re-design the road to switch the trail and sidewalk at this late stage.**

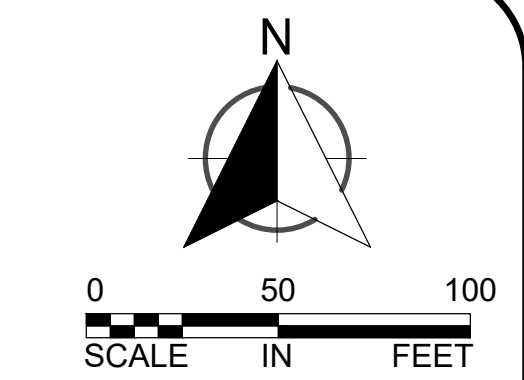
September 4, 2024
Dayton Creek Addition
Jon Sevald

End of Comments



LEGEND

- SANITARY MANHOLE
- STORM MANHOLE
- CATCH BASIN
- FLARED END SECTION
- OVERFLOW CONTROL STRUCTURE
- LIGHT POLE
- UTILITY POLE
- GUY WIRE
- ELECTRIC HANDHOLE
- SIGN
- TRAFFIC CONTROL
- POST / BOLLARD
- SANITARY LINE
- STORM LINE
- UNDERGROUND POWER
- OVERHEAD UTILITY
- TREE (SAP* = TREE SAPLING)
- FOUND IRON SURVEY MONUMENT
- SET CAPPED REBAR RLS 49003
- BITUMINOUS
- CONCRETE



BENCH MARK

MNDOT MONUMENT
BROCK MN053
NAD83(1986)
ELEVATION = 949.15

GENERAL NOTES

- (GN1) Location, sizes and types of underground utilities shown are a combination of observed evidence and plan information, together with field markings by those utility locators that responded to Gopher State One Call Ticket No. 230750233. However, lacking excavation, the exact location of underground features cannot be accurately, completely, and reliably depicted. WSB & Associates makes no guarantee that the utilities shown comprise all of the utilities in the area. Where additional or more detailed information is required, the client is advised that excavation and/or a private utility locate request may be necessary.
- (GN2) Easements shown are as identified in Old Republic National Title Insurance Company Commitment No. 200815 C-1, with a commitment date of March 23, 2023. No other search of the public records for easements or encumbrances was made by the undersigned.
- (GN3) Wetlands shown as delineated by WSB LLC and approved by TEP on August 30, 2023.
- (GN4) Per client's instructions, said survey only includes the boundaries as described in Parcels 1 and 3 of the referenced commitment. Parcels 2, 4 and 5 are shown for informational purposes only and are not a part of the overall boundary of the survey.

PROPOSED LOT DATA

Number of Proposed Lots = 0
Number of Proposed Outlots = 13

Outlot A = 0.56 acres
Outlot B = 2.38 acres
Outlot C = 10.46 acres
Outlot D = 1.63 acres
Outlot E = 4.07 acres
Outlot F = 4.25 acres
Outlot G = 2.96 acres
Outlot H = 7.68 acres
Outlot I = 0.22 acres
Outlot J = 1.77 acres
Outlot K = 0.20 acres
Outlot L = 2.21 acres
Outlot M = 10.35 acres

Dedicated Right of Way = 3.14 acres
Total Area = 51.88 acres

PRELIMINARY PLAT

Owner / Developer: Schany Group, LLC
22140 Oakdale Drive
Rogers, MN 55374

Preliminary Plat prepared on May 1, 2023
Revised on March 20, 2024
Revised on July 18, 2024
Revised on September 25, 2024

SURVEYOR'S CERTIFICATION

I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this ____ day of ____, 2024.

Jeffrey J. Rolfsen, PS, Minnesota License No. 49003

| | |
|-------------------------------------|---------------------|
| Client: SCHANY GROUP, LLC | |
| Job No. 022318 | |
| Surveyed By: JS | |
| Drawn By: JAR | |
| Checked By: JAR | |
| Scale: 1" = 50' | CAD No. 022318 V-PP |
| Date: 05/20/2024 | |



DAYTON CREEK ADDITION
PRELIMINARY PLAT
PART OF OUTLOTS A & B
BROCKTON CROSSING
HENNEPIN COUNTY, MINNESOTA

PROPERTY DESCRIPTION

(Per Old Republic National Title Insurance Company Informational Commitment No. 200815 C-1, with a commitment date of May 15, 2023)
NOTE: Per client's instructions, said survey only includes the boundaries as described in Parcels 1 and 3 of the above commitment. Parcels 2, 4 and 5 are shown for informational purposes only and are not a part of the overall boundary of the survey.)

Parcel 1:

Outlot A, Brockton Crossing, Hennepin County, Minnesota, except that part described as follows:
That part of said Outlot A described as beginning at the southeast corner of said Outlot A; thence North 21 degrees 20 minutes 58 seconds West, assumed bearing, along the westerly line of said Outlot A a distance of 126.07 feet to the point of beginning; thence South 88 degrees 50 minutes 41 seconds East a distance of 289.57 feet; thence North 88 degrees 50 minutes 39 seconds West a distance of 483.55 feet; thence South 01 degree 09 minutes 21 seconds West a distance of 8.00 feet to the southerly line of said Outlot A; thence easterly, southerly, easterly and northeasterly along the southerly lines of said Outlot A to the point of beginning.

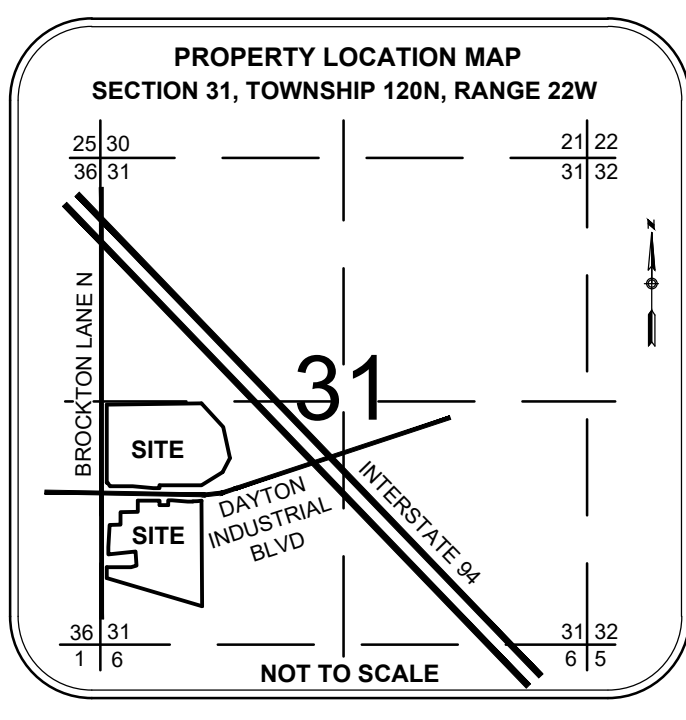
AND

That part of Dayton Industrial Boulevard as dedicated on the plat of Brockton Crossing, according to the recorded plat thereof, Hennepin County, Minnesota, which lies northwesterly of the following described line:
Commencing at the most easterly corner of Outlot A of said plat; thence North 21 degrees 20 minutes 58 seconds West, along the northeasterly line of said Outlot A a distance of 90.05 feet to the point of beginning of the line to be described; thence North 28 degrees 12 minutes 41 seconds East a distance of 182.06 feet to the northeasterly line of said Dayton Industrial Boulevard and there terminating.

Parcel 3:

Outlot B, Brockton Crossing, Hennepin County, Minnesota, except those parts described as follows:
That part of said Outlot B described as commencing at the southwest corner of said Outlot B; thence North 01 degree 09 minutes 19 seconds East, assumed bearing, along the west line of said Outlot B a distance of 126.07 feet to the point of beginning; thence South 88 degrees 50 minutes 41 seconds East a distance of 281.06 feet; thence North 74 degrees 03 minutes 42 seconds East a distance of 66.90 feet; thence North 03 degrees 14 minutes 45 seconds West a distance of 180.69 feet; thence South 85 degrees 36 minutes 02 seconds West, a distance of 316.15 feet; thence North 01 degree 09 minutes 19 seconds East a distance of 85.08 feet; thence North 04 degrees 58 minutes 11 seconds East a distance of 254.52 feet; thence South 89 degrees 45 minutes 01 seconds East a distance of 123.08 feet; thence North 01 degrees 09 minutes 21 seconds East a distance of 145.00 feet; thence North 89 degrees 45 minutes 01 seconds East a distance of 202.25 feet; thence North 01 degrees 09 minutes 21 seconds East a distance of 131.21 feet to the northerly line of said Outlot B; thence westerly, southwesterly, southerly, westerly and southerly along the northerly, northwesterly and westerly lines of said Outlot B to the point of beginning.

And except that part of said Outlot B described as commencing at the most northerly northwest corner of said Outlot B; thence South 88 degrees 50 minutes 39 seconds East, along the northerly line of said Outlot B, a distance of 548.20 feet to an angle point in said northerly line; thence South 01 degree 09 minutes 21 seconds West, along said northerly line, a distance of 22.00 feet to an angle point in said northerly line and the point of beginning; thence South 88 degrees 50 minutes 39 seconds East, along the northerly line of said Outlot B, a distance of 100.00 feet to an angle point in said northerly line; thence South 01 degree 09 minutes 21 seconds West a distance of 30.00 feet; thence North 88 degrees 50 minutes 39 seconds West a distance of 100.00 feet to the intersection with a line bearing South 01 degree 09 minutes 21 seconds West from the point of beginning; thence North 01 degree 09 minutes 21 seconds East a distance of 30.00 feet to the point of beginning.



SEE PAGE 3

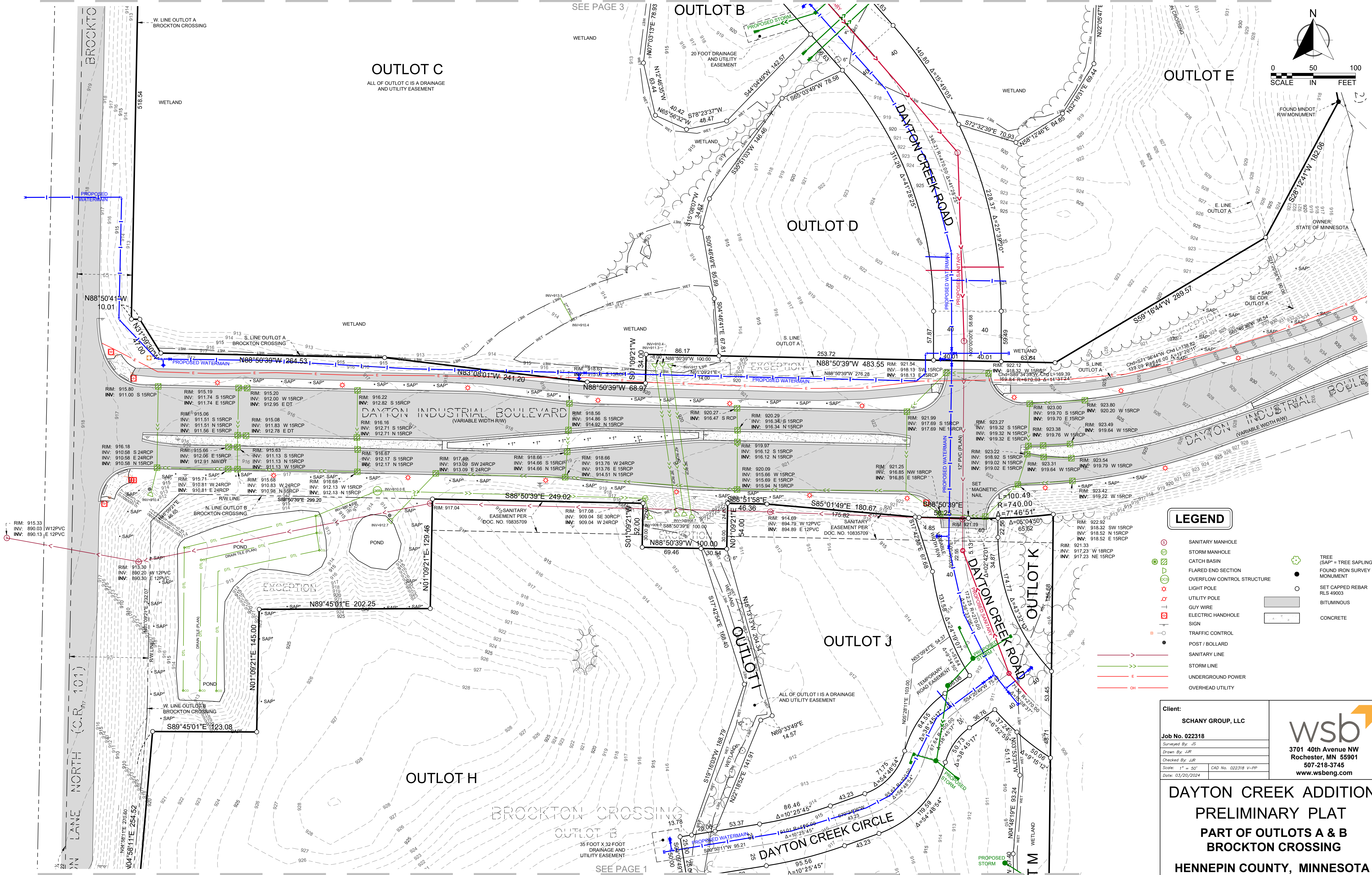
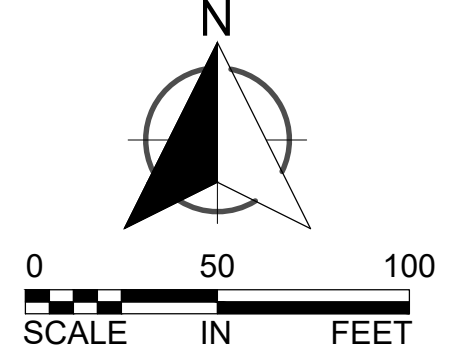
OUTLOT B

OUTLOT E

OUTLOT D

OUTLOT C

ALL OF OUTLOT C IS A DRAINAGE
AND UTILITY EASEMENT



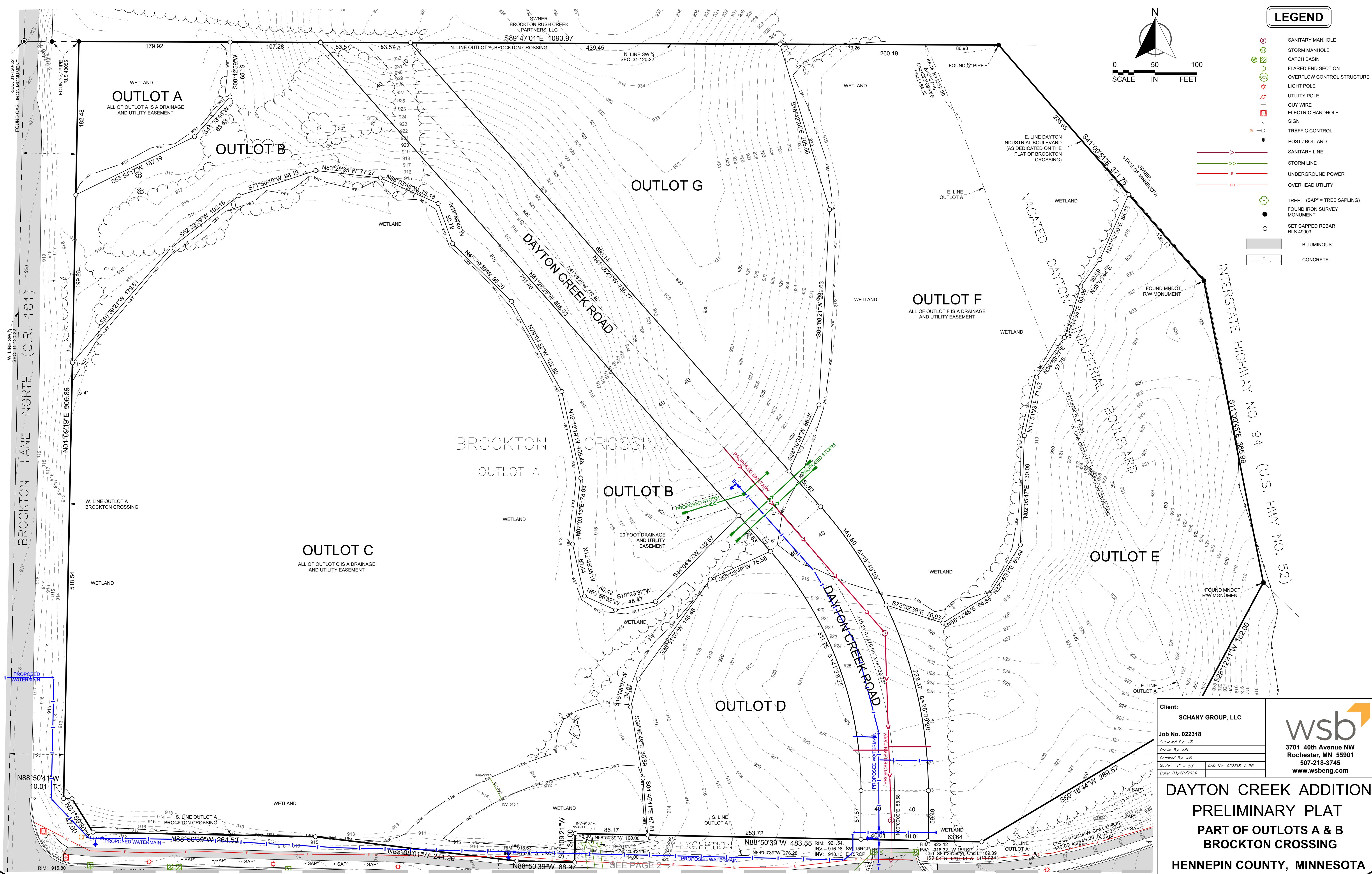
LEGEND

- SANITARY MANHOLE
- STORM MANHOLE
- CATCH BASIN
- FLARED END SECTION
- OVERFLOW CONTROL STRUCTURE
- LIGHT POLE
- UTILITY POLE
- GUY WIRE
- ELECTRIC HANDHOLE
- SIGN
- TRAFFIC CONTROL
- POST / BOLLARD
- SANITARY LINE
- STORM LINE
- UNDERGROUND POWER
- OVERHEAD UTILITY
- TREE (SAP* = TREE SAPLING)
- FOUND IRON SURVEY MONUMENT
- SET CAPPED REBAR RLS 49003
- BITUMINOUS
- CONCRETE

Client: SCHANY GROUP, LLC
Job No. 022318
Surveyed By: JS
Drawn By: JAR
Checked By: JAR
Scale: 1" = 50' CAD No. 022318 V-PP
Date: 03/20/2024

wsb
3701 40th Avenue NW
Rochester, MN 55901
507-218-3745
www.wsbg.com

DAYTON CREEK ADDITION
PRELIMINARY PLAT
PART OF OUTLOTS A & B
BROCKTON CROSSING
HENNEPIN COUNTY, MINNESOTA



LEGEND

- SANITARY MANHOLE
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- FOUND IRON SURVEY MONUMENT
- SET CAPPED REBAR RLS 49003
- BITUMINOUS
- CONCRETE

Client: SCHANY GROUP, LLC

Job No. 022318

Surveyed By: JS

Drawn By: JAR

Checked By: JAR

Scale: 1" = 50' CAD No. 022318 V-PP

Date: 03/20/2024

wsb

3701 40th Avenue NW
Rochester, MN 55901
507-218-3745
www.wsbeng.com

DAYTON CREEK ADDITION
PRELIMINARY PLAT
PART OF OUTLOTS A & B
BROCKTON CROSSING
HENNEPIN COUNTY, MINNESOTA

DAYTON CREEK ADDITION

C.R. DOC. NO. _____

KNOW ALL PERSONS BY THESE PRESENTS: That Schany Group, LLC, a Minnesota limited liability limited partnership, owner of the following described property:

Outlot A, Brockton Crossing, Hennepin County, Minnesota, except that part described as follows:

That part of said Outlot A described as beginning at the southeast corner of said Outlot A; thence North 21 degrees 20 minutes 58 seconds West, assumed bearing, along the easterly line of said Outlot A a distance of 90.06 feet; thence South 59 degrees 16 minutes 44 seconds West a distance of 289.57 feet; thence North 88 degrees 50 minutes 39 seconds West a distance of 483.55 feet; thence South 01 degree 09 minutes 21 seconds West a distance of 8.00 feet to the southerly line of said Outlot A; thence easterly, southerly, easterly and northeasterly along the southerly lines of said Outlot A to the point of beginning.

AND

That part of Dayton Industrial Boulevard as dedicated on the plat of Brockton Crossing, according to the recorded plat thereof, Hennepin County, Minnesota, which lies northwesterly of the following described line:

Commencing at the most easterly corner of Outlot A of said plat; thence North 21 degrees 20 minutes 58 seconds West, along the northeasterly line of said Outlot A a distance of 90.05 feet to the point of beginning of the line to be described; thence North 28 degrees 12 minutes 41 seconds East a distance of 182.06 feet to the northeasterly line of said Dayton Industrial Boulevard and there terminating.

AND

Outlot B, Brockton Crossing, Hennepin County, Minnesota, except those parts described as follows:

That part of said Outlot B described as commencing at the southwest corner of said Outlot B; thence North 01 degree 09 minutes 19 seconds East, assumed bearing, along the west line of said Outlot B a distance of 126.07 feet to the point of beginning; thence South 88 degrees 50 minutes 41 seconds East a distance of 281.06 feet; thence North 74 degrees 03 minutes 42 seconds East a distance of 66.90 feet; thence North 03 degrees 14 minutes 45 seconds West a distance of 160.69 feet; thence South 85 degrees 36 minutes 02 seconds West, a distance of 316.15 feet; thence North 01 degree 09 minutes 19 seconds East a distance of 85.08 feet; thence North 04 degrees 58 minutes 11 seconds East a distance of 254.52 feet; thence South 89 degrees 45 minutes 01 seconds East a distance of 123.08 feet; thence North 01 degrees 09 minutes 21 seconds East a distance of 145.00 feet; thence North 89 degrees 45 minutes 01 seconds East a distance of 202.25 feet; thence North 01 degrees 09 minutes 21 seconds East a distance of 131.21 feet to the northerly line of said Outlot B; thence westerly, southwesterly, southerly, westerly and southerly along the northerly, northwesterly and westerly lines of said Outlot B to the point of beginning.

And except that part of said Outlot B described as commencing at the most northerly northwest corner of said Outlot B; thence South 88 degrees 50 minutes 39 seconds East, along the northerly line of said Outlot B, a distance of 548.20 feet to an angle point in said northerly line; thence South 01 degree 09 minutes 21 seconds West, along said northerly line, a distance of 22.00 feet to an angle point in said northerly line and the point of beginning; thence South 88 degrees 50 minutes 39 seconds East, along the northerly line of said Outlot B, a distance of 100.00 feet to an angle point in said northerly line; thence South 01 degree 09 minutes 21 seconds West a distance of 30.00 feet; thence North 88 degrees 50 minutes 39 seconds West a distance of 100.00 feet to the intersection with a line bearing South 01 degree 09 minutes 21 seconds West from the point of beginning; thence North 01 degree 09 minutes 21 seconds East a distance of 30.00 feet to the point of beginning.

Has caused the same to be surveyed and platted as DAYTON CREEK ADDITION, and does hereby dedicate to the public for public use, the public ways as created by this plat.

In witness whereof said Schany Group, LLC, a Minnesota limited liability limited partnership, has caused these presents to be signed by its proper officer this _____ day of _____, 20____.

Signed: Schany Group, LLC

XXXXXXXX, XXXXXX

STATE OF MINNESOTA

COUNTY OF _____

This instrument was acknowledged before me this _____ day of _____, 20____, by XXXXXXX, XXXXXX of Schany Group, LLC, a Minnesota limited liability company, on behalf of the company.

_____, Notary Signature _____, Notary Printed Name

Notary Public, _____ County, Minnesota

My Commission Expires _____

I, Jeffrey J. Rolfsen do hereby certify that this plat was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on this plat have been, or will be correctly set within one year; that all water boundaries and wet lands, as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of this certificate are shown and labeled on this plat; and all public ways are shown and labeled on this plat.

Dated this _____ day of _____, 20____.

Jeffrey J. Rolfsen, Licensed Land Surveyor
Minnesota License No. 49003

STATE OF MINNESOTA

COUNTY OF _____

This instrument was acknowledged before me this _____ day of _____, 20____ by Jeffrey J. Rolfsen.

_____, Notary Signature _____, Notary Printed Name

Notary Public, _____ County, Minnesota

My Commission Expires _____

CITY COUNCIL, CITY OF DAYTON, MINNESOTA

This plat of DAYTON CREEK ADDITION was approved and accepted by the City Council of the City of Dayton, Minnesota, at a regular meeting thereof held this _____ day of _____, 20____, and said plat is in compliance with the provisions of Minnesota Statutes, Section 505.03, Subdivision 2.

City Council, City of Dayton, Minnesota

By: _____ Mayor By: _____ Clerk

COUNTY AUDITOR, Hennepin County, Minnesota

I hereby certify that taxes payable in 20____ and prior years have been paid for land described on this plat, dated this _____ day of _____, 20____.

Daniel Rogan, County Auditor

By: _____, Deputy

SURVEY DIVISION, Hennepin County, Minnesota

Pursuant to Minnesota Statutes Section 383B.565 (1969), this plat has been approved this _____ day of _____, 20____.

Chris F. Mavis, County Surveyor

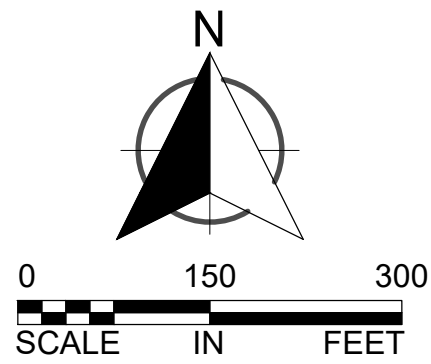
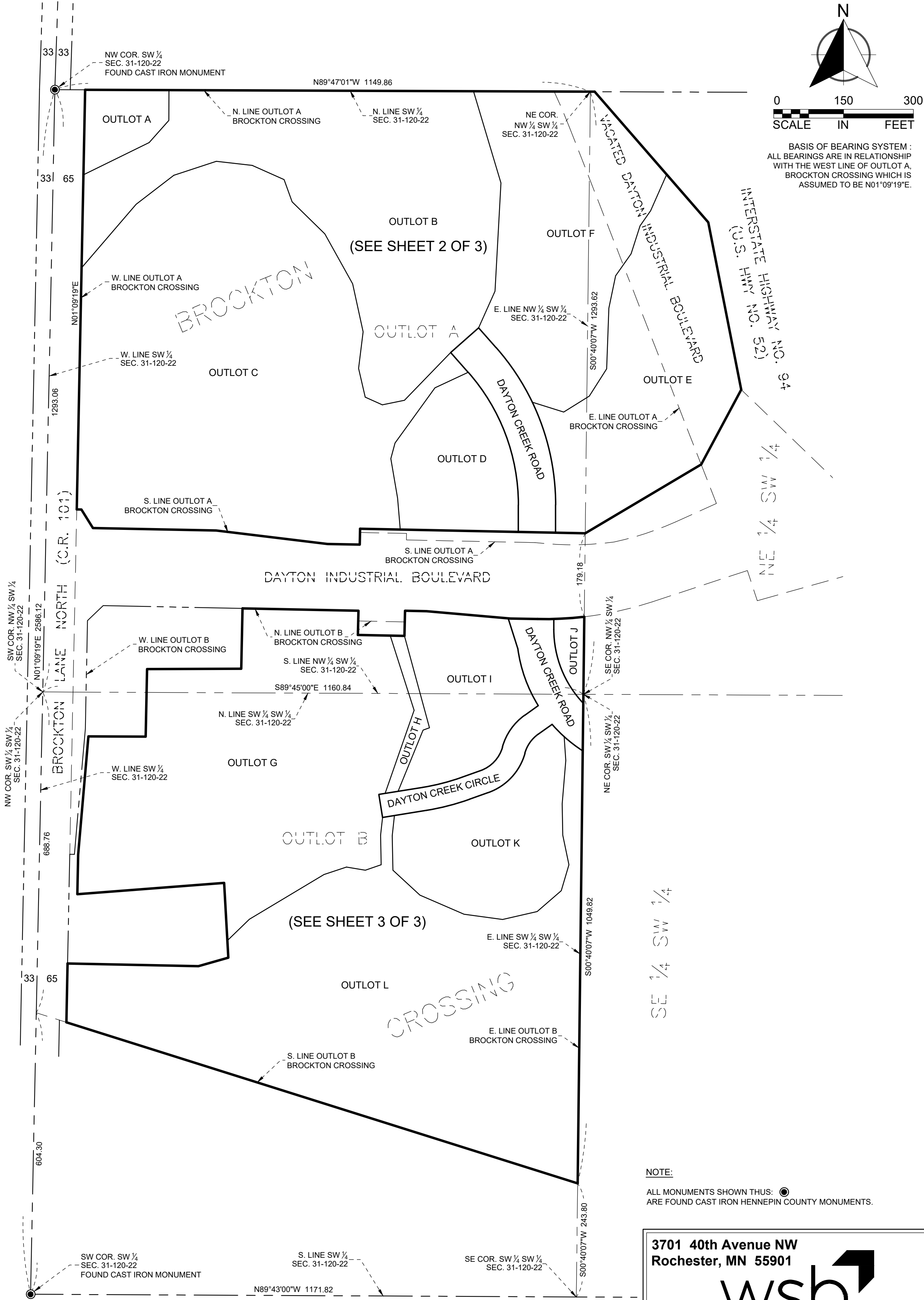
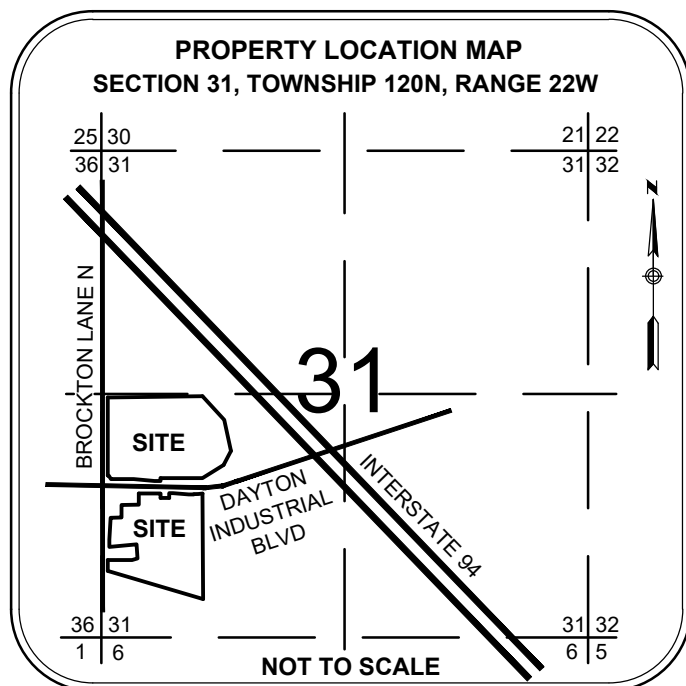
By: _____

COUNTY RECORDER, Hennepin County, Minnesota

I hereby certify that the within plat of DAYTON CREEK ADDITION was filed in this office this _____ day of _____, 20____, at _____ o'clock ____ M.

XXXXXXXX, County Recorder

By: _____, Deputy



BASIS OF BEARING SYSTEM :
ALL BEARINGS ARE IN RELATIONSHIP
WITH THE WEST LINE OF OUTLOT A,
BROCKTON CROSSING WHICH IS
ASSUMED TO BE N01°09'19\"/>

NOTE:

ALL MONUMENTS SHOWN THUS: ●
ARE FOUND CAST IRON HENNEPIN COUNTY MONUMENTS.

3701 40th Avenue NW
Rochester, MN 55901

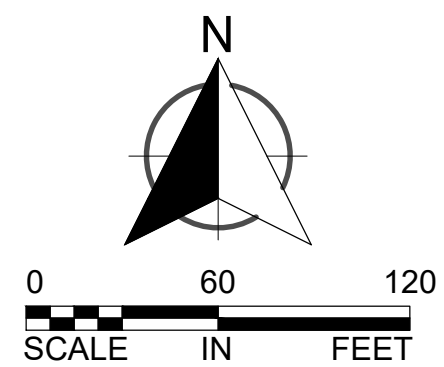
wsb

507-218-3745

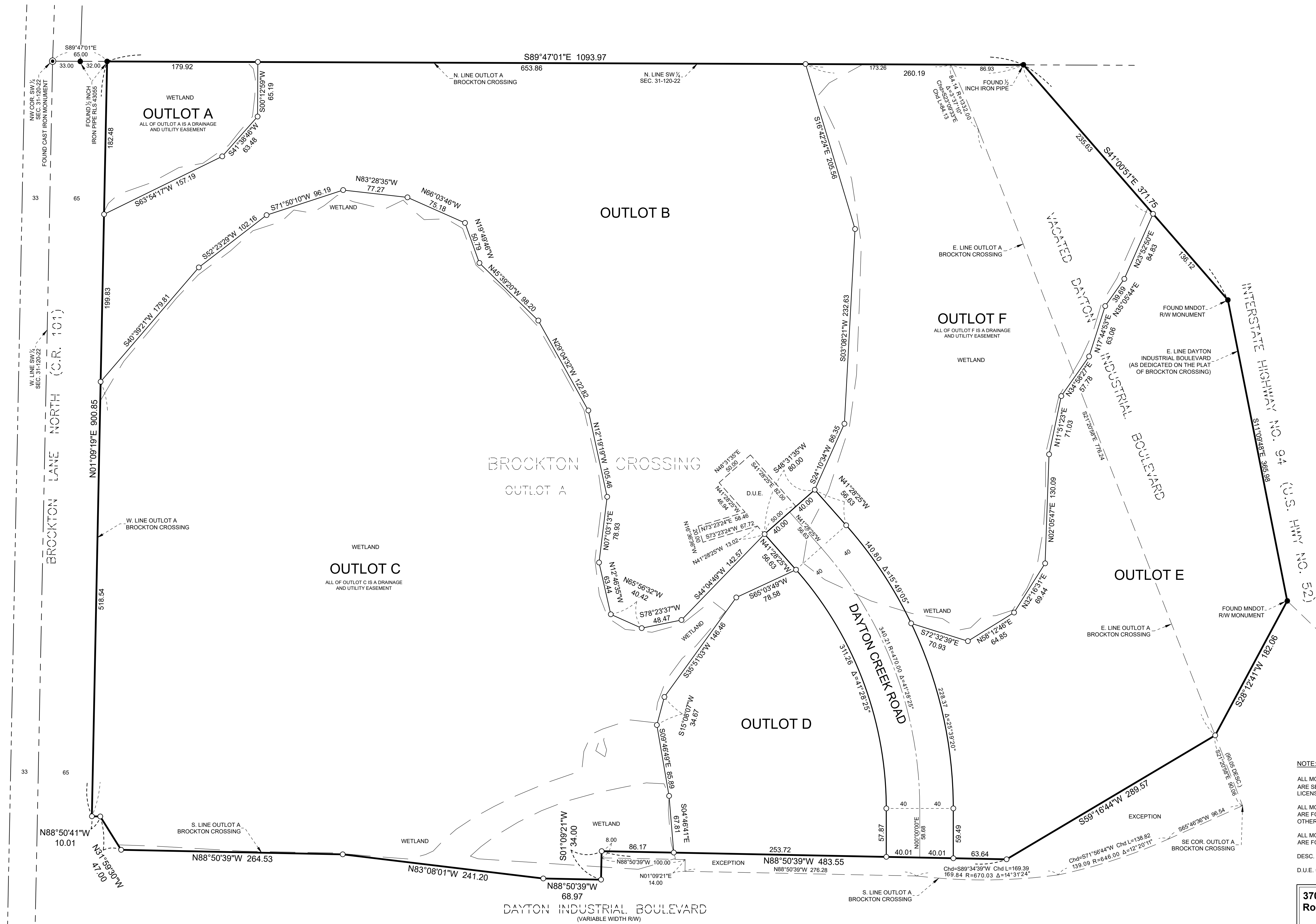
www.ws beng.com

DAYTON CREEK ADDITION

C.R. DOC. NO. _____



BASIS OF BEARING SYSTEM:
ALL BEARINGS ARE IN RELATIONSHIP
WITH THE WEST LINE OF OUTLOT A,
BROCKTON CROSSING WHICH IS
ASSUMED TO BE N01°09'19"E.



SEE SHEET 3 OF 3 SHEETS

3701 40th Avenue NW
Rochester, MN 55901

wsb

507-218-3745
www.wsbeng.com

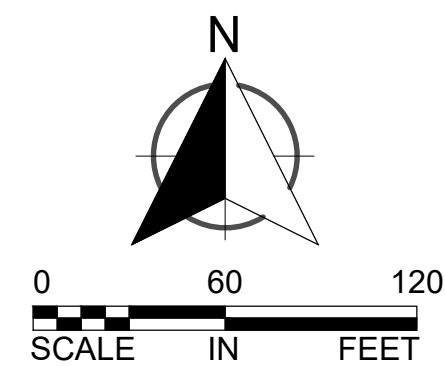
SHEET 2 OF 3 SHEETS

022318

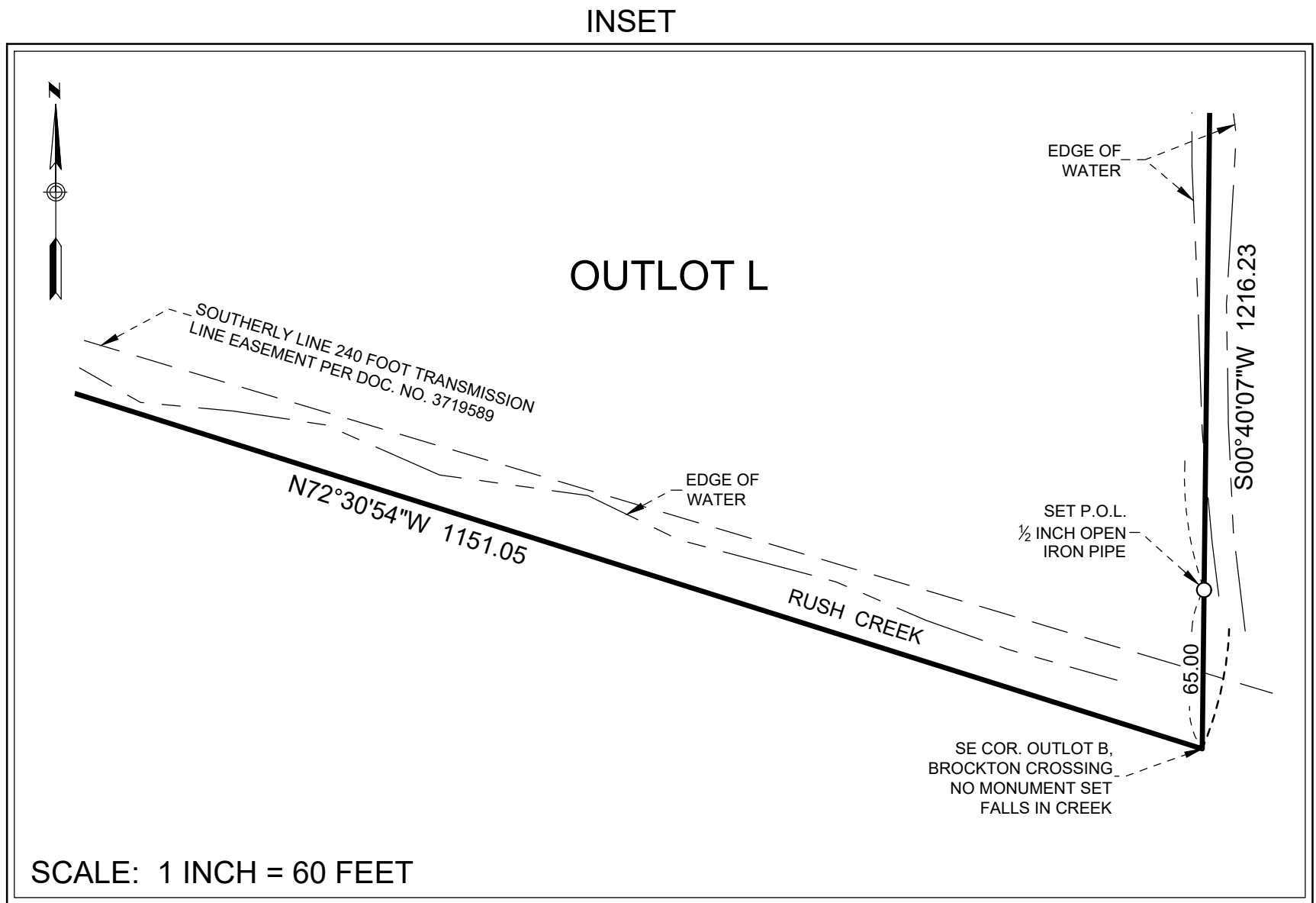
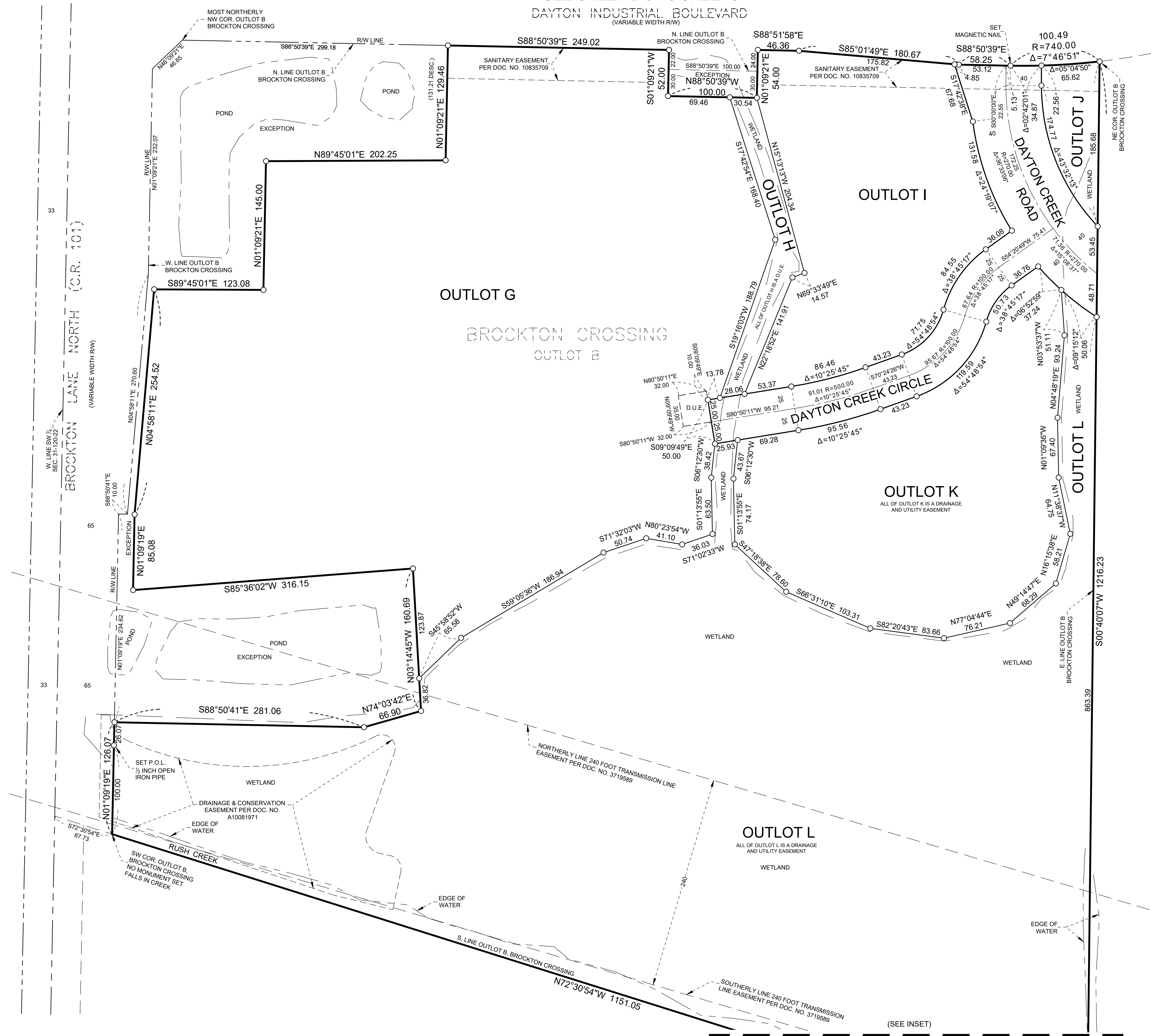
DAYTON CREEK ADDITION

SEE SHEET 2 OF 3 SHEETS
DAYTON INDUSTRIAL BOULEVARD
(VARIABLE WIDTH RW)

C.R. DOC. NO. _____



BASIS OF BEARING SYSTEM:
ALL BEARINGS ARE IN RELATIONSHIP
WITH THE WEST LINE OF OUTLOT A.
BROCKTON CROSSING WHICH IS
ASSUMED TO BE N01°09'19"E.



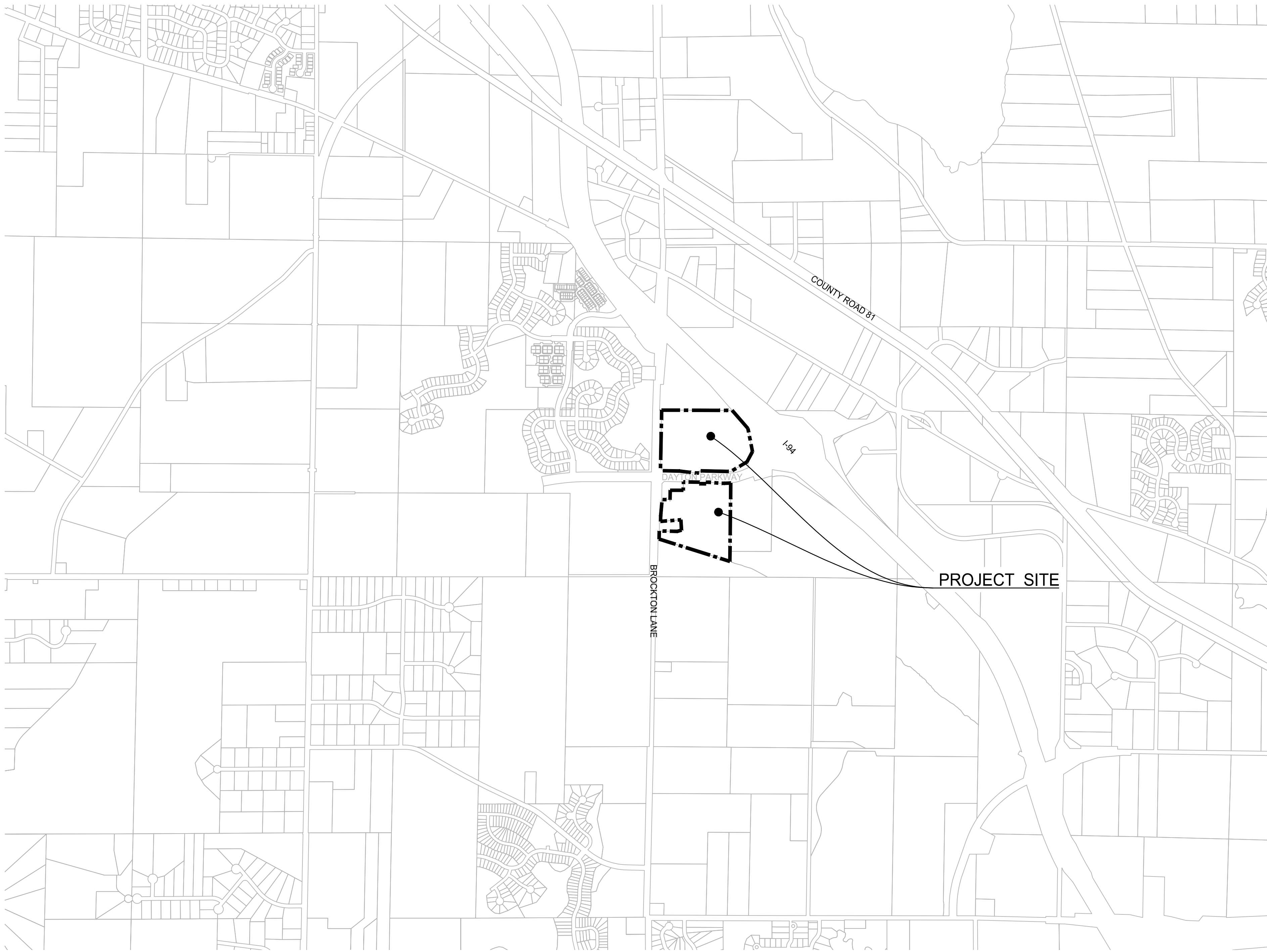
NOTE:
ALL MONUMENTS SHOWN THUS: ○
ARE SET 3/8 INCH BY 14 INCH CAPPED IRON REBAR MONUMENTS WITH
LICENSE NO. 49003, UNLESS OTHERWISE NOTED.
ALL MONUMENTS SHOWN THUS: ●
ARE FOUND 1/2 INCH OPEN IRON PIPE MONUMENTS UNLESS
OTHERWISE NOTED.
DESC. = DESCRIBED
D.U.E. = DRAINAGE AND UTILITY EASEMENT

3701 40th Avenue NW
Rochester, MN 55901
wsb
507-218-3745
www.wsbeng.com

DAYTON CREEK ADDITION - SCHANY GROUP, LLC

City of Dayton, Minnesota

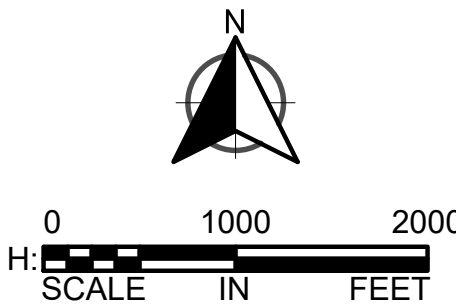
WSB Project Number 022318-000
Issue Date 08/22/2024; RESUBMITTED FOR PRELIMINARY PLAT SUBMITTAL



1 PROJECT LOCATION MAP
1 SCALE: NOT TO SCALE

THIS PLAN SET HAS BEEN PREPARED FOR:

SCHANY GROUP, LLC
22140 OAKDALE DRIVE
ROGERS, MN 55374



| Sheet List Table | |
|------------------|--------------------------------------|
| Sheet Number | Sheet Title |
| 1 | TITLE SHEET |
| 2 | GENERAL LAYOUT 1 |
| 3 | TYP SECTION |
| 4 | MISC DETAILS 1 |
| 5 | MISC DETAILS 2 |
| 6 | MISC DETAILS 3 |
| 7 | MISC DETAILS 4 |
| 8 | MISC DETAILS 5 |
| 9 | STANDARD PLANS 1 |
| 10 | STANDARD PLANS 2 |
| 11 | STANDARD PLANS 3 |
| 12 | STANDARD PLANS 4 |
| 13 | STANDARD PLANS 5 |
| 14 | STANDARD PLANS 6 |
| 15 | OVERALL EXISTING CONDITIONS |
| 16 | EXISTING CONDITIONS - NORTH |
| 17 | EXISTING CONDITIONS - SOUTH |
| 18 | REMOVAL PLAN 1 |
| 19 | PRELIMINARY SITE PLAN |
| 20 | SANITARY SEWER AND WATERMAIN PLANS 1 |
| 21 | SANITARY SEWER AND WATERMAIN PLANS 2 |
| 22 | SANITARY SEWER AND WATERMAIN PLANS 3 |
| 23 | SANITARY SEWER AND WATERMAIN PLANS 4 |
| 24 | SANITARY SEWER AND WATERMAIN PLANS 5 |
| 25 | STREET AND STORM PLAN AND PROFILE |
| 26 | STREET AND STORM PLAN AND PROFILE |
| 27 | STREET AND STORM PLAN AND PROFILE |
| 28 | EROSION CONTROL PLAN AND PLAN 1 |
| 29 | EROSION CONTROL PLAN AND PLAN 2 |
| 30 | SIGNING AND STRIPING PLAN 1 |
| 31 | SIGNING AND STRIPING PLAN 2 |
| 32 | NORTH GRADING PLAN |
| 33 | SOUTH GRADING PLAN |

GENERAL NOTES

1. THE 2022 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN.
2. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING THE LATEST FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS.

THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE DRAWINGS CONCERNING TYPE AND LOCATION OF PRIVATE UTILITIES HAS BEEN DESIGNATED UTILITY QUALITY LEVEL D. THESE UTILITY QUALITY LEVELS WAS DETERMINED ACCORDING TO THE GUIDELINES OF C/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR IS TO DETERMINE THE TYPE AND LOCATION OF PRIVATE UTILITIES AS MAY BE DEEMED NECESSARY TO AVOID DAMAGE THERETO.



SCALE: AS SHOWN
DESIGN BY: KH
PLAN BY: IE
CHECK BY: PS

| REVISIONS | | NO. | DATE | DESCRIPTION |
|-----------|-----------|-----|------|------------------------|
| 1 | 7/19/2024 | | | Revised City Submittal |
| 2 | 8/22/2024 | | | Revised City Submittal |
| 3 | 9/25/2024 | | | Revised City Submittal |

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

VICKI J. VAN DELL
DATE: 08/22/2024 LIC. NO. 41352

TITLE SHEET

DAYTON CREEK ADDITION
SCHANY GROUP, LLC

WSB PROJECT NO.
022318-000

SCALE: AS SHOWN
DESIGN BY: XXX
PLAN BY: XXX
CHECK BY: XXX

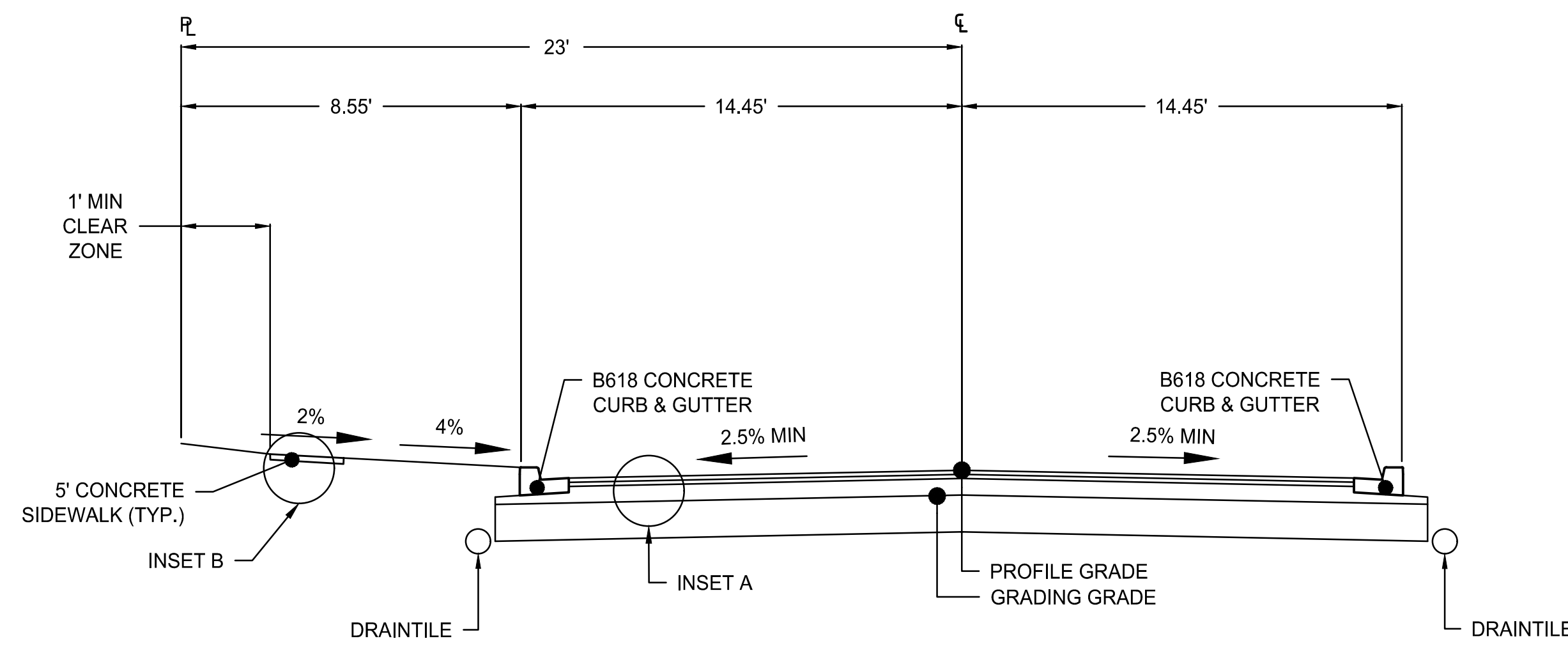
| REVISIONS | | NO. | DATE | DESCRIPTION |
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VICKI J. VAN DELL
DATE: 08/22/2024 LIC. NO. 41352

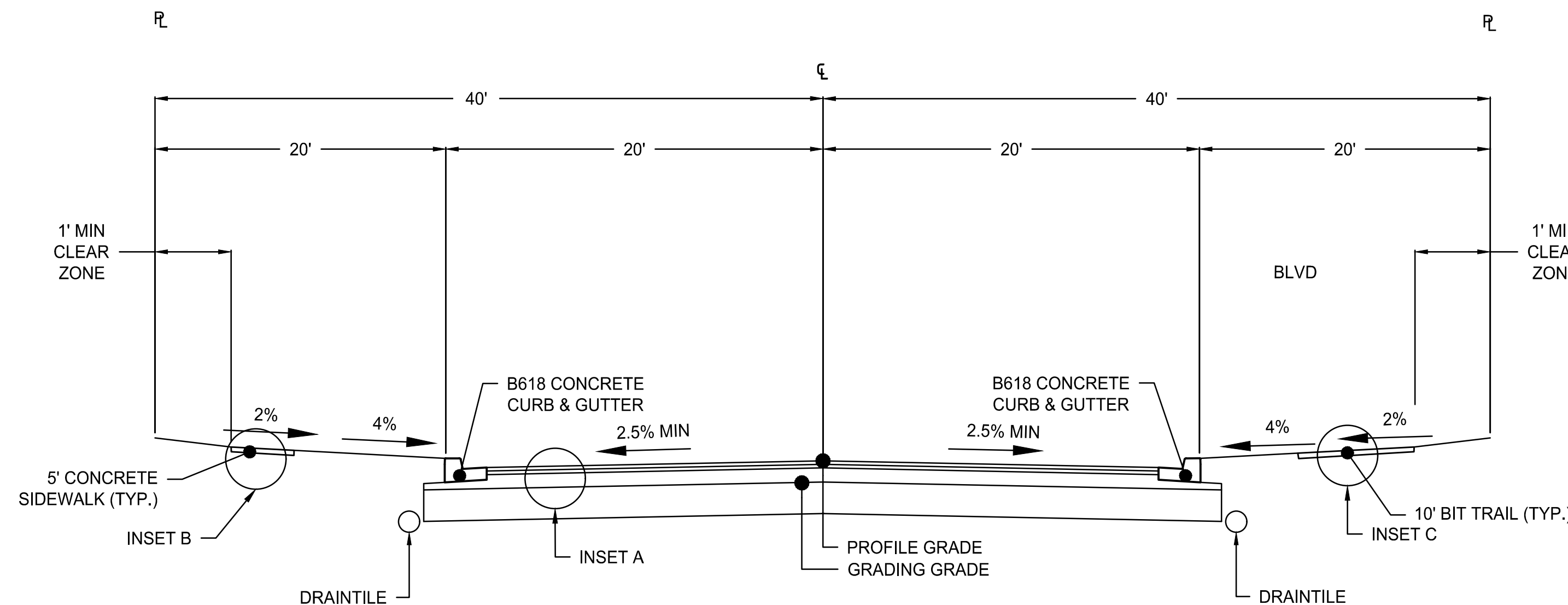
TYPICAL SECTIONS

DAYTON CREEK ADDITION
SCHANY GROUP, LLC



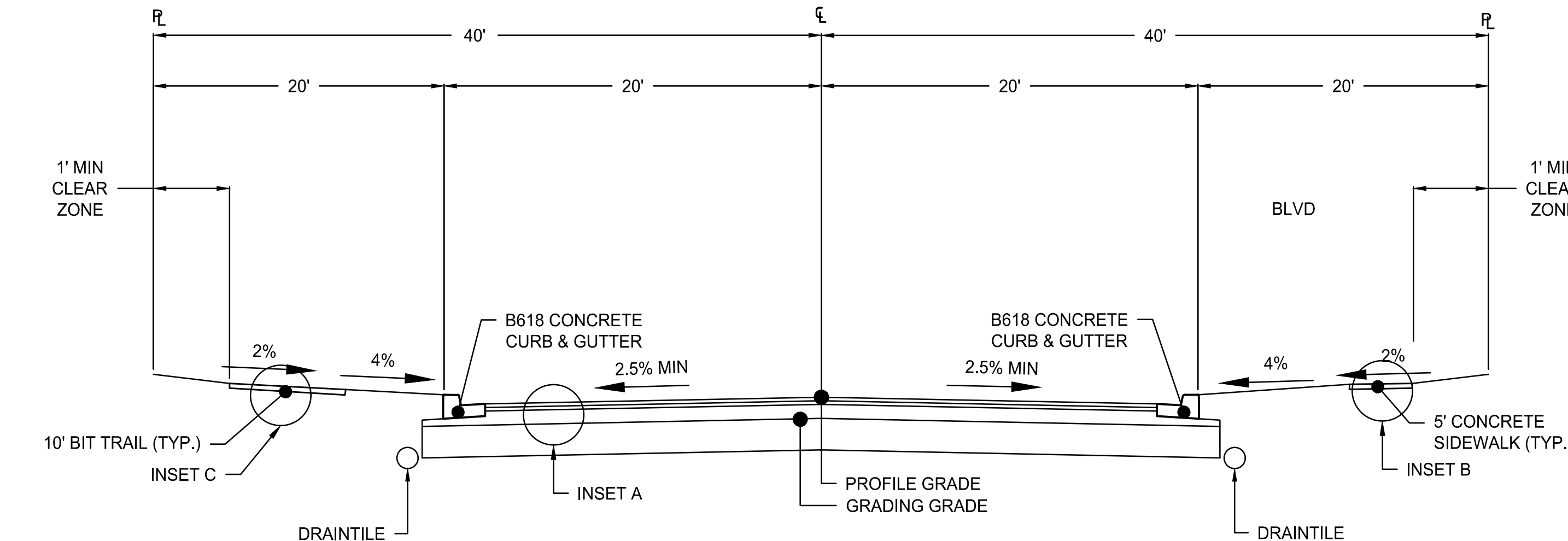
SOUTH E-W ROAD

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NOT TO SCALE



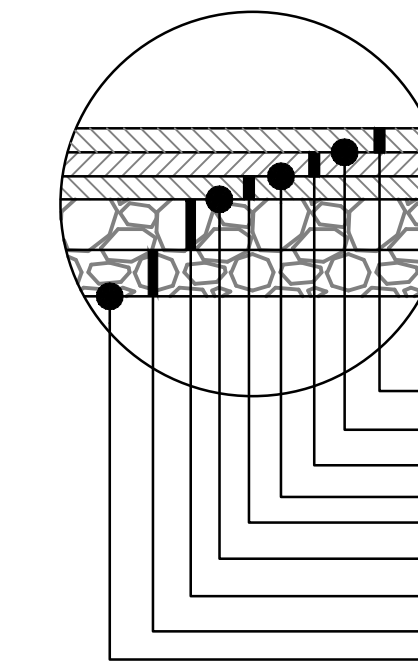
NORTH ROAD

11+45.73 - 15+37.11
NOT TO SCALE



SOUTH ROAD

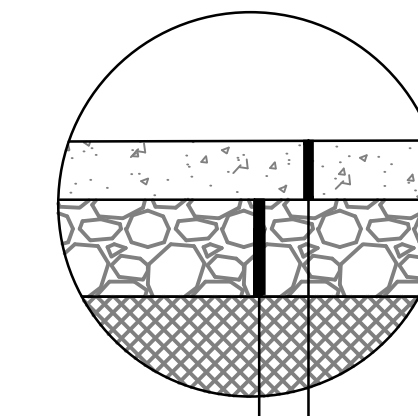
21+11.40 - 22+46.37
NOT TO SCALE



INSET A

URBAN COLLECTOR STREET

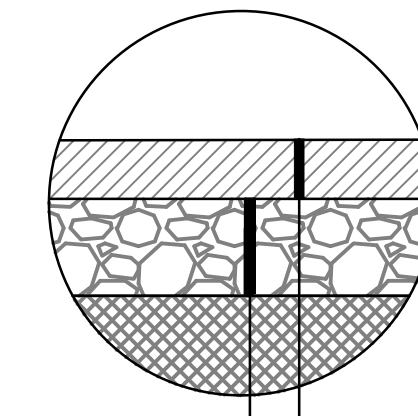
- 2" TYPE SPWEA340C BITUMINOUS WEAR COURSE
- BITUMINOUS TACK COAT (2357)
- 2" TYPE SPWEA340C BITUMINOUS WEAR COURSE
- BITUMINOUS TACK COAT (2357)
- 2" TYPE SPNWB330C BITUMINOUS NON-WEAR COURSE
- BITUMINOUS TACK COAT (2357)
- 12" CLASS 5 AGGREGATE BASE (MNDOT 3138, 100% CRUSHED)
- 12" MINIMUM SELECT GRANULAR BORROW (MNDOT 3149.2.B.2)
- GEOTEXTILE FABRIC TYPE V NON-WOVEN



INSET B

CONCRETE SIDEWALK
(CITY STD. PLATE NO. STR-09)

- 6.0" CONCRETE (3F52)
- 6.0" CLASS 5 AGGREGATE BASE OR RECYCLED EQUAL (MNDOT 3138, 100% CRUSHED)

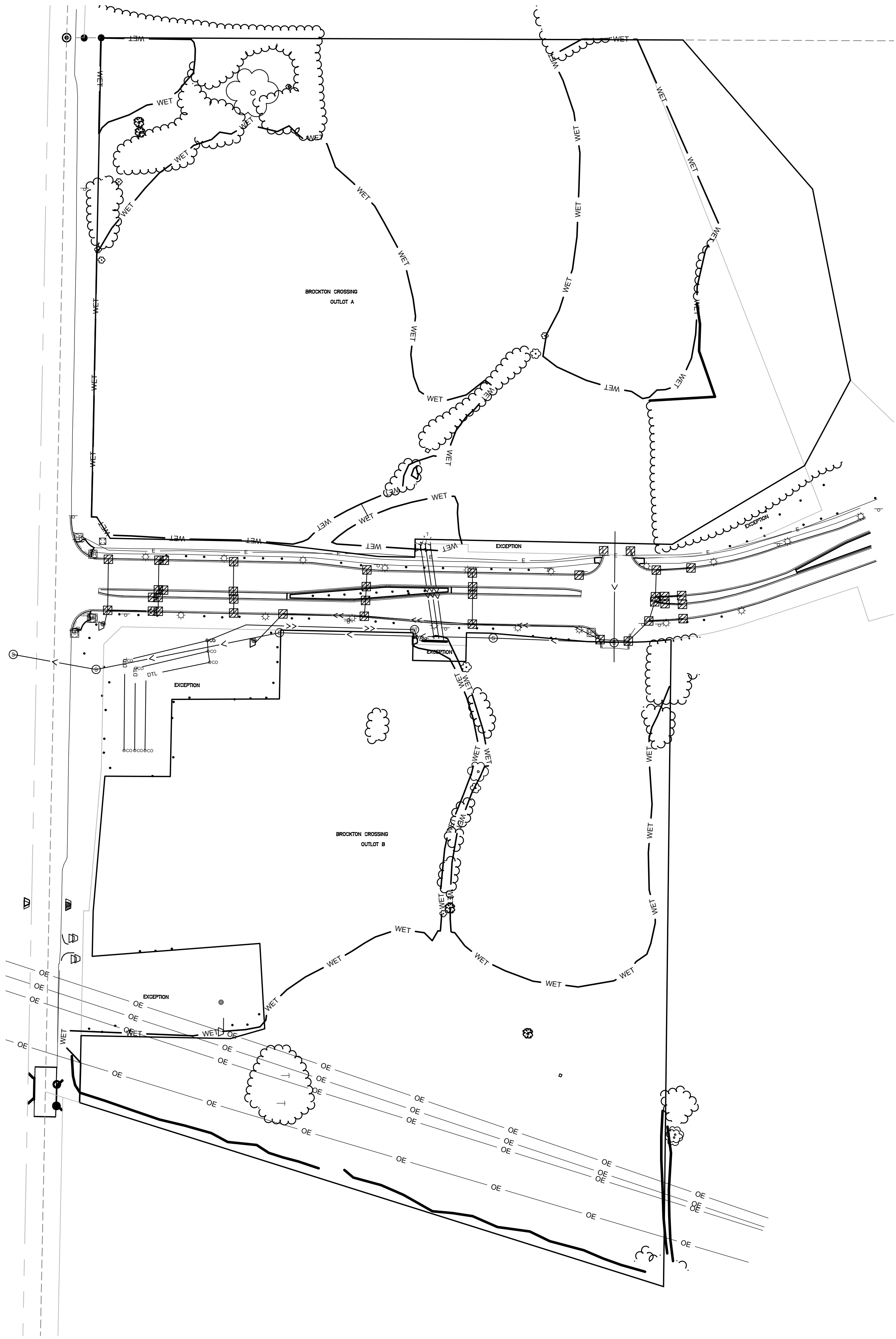


INSET C

BITUMINOUS TRAIL
(CITY STD. PLATE NO. STR-19)

- 3.0" SPWEA240B BITUMINOUS WEAR COURSE
- 6.0" CLASS 5 AGGREGATE BASE 100% CRUSHED

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LEGEND

- SECTION LINE
- RIGHT-OF-WAY LINE
- PROPERTY LINE
- ADJACENT PROPERTY LINE
- EXISTING EASEMENT - RIGHT-OF-WAY
- EXISTING EASEMENT - TRANSMISSION
- EXISTING CONTOUR (MAJOR)
- EXISTING CONTOUR (MINOR)
- LIGHT POLE
- UTILITY POLE
- GUY WIRE
- ELECTRIC HANDHOLE
- SIGN
- TRAFFIC CONTROL
- POST / BOLLARD
- TREE
- SAP" = TREE SAPLING
- FOUND IRON SURVEY MONUMENT
- EXISTING WATER EDGE
- WETLAND EDGE
- EXISTING CURB AND GUTTER
- EXISTING CONCRETE (EDGE)
- EXISTING BITUMINOUS (EDGE)
- EXISTING GRAVEL (EDGE)
- EXISTING SANITARY SEWER MAIN
- EXISTING SANITARY STRUCTURE
- EXISTING STORM SEWER PIPE
- EXISTING STORM STRUCTURE
- EXISTING OUTLET CONTROL STRUCTURE
- EXISTING WATERMAIN
- EXISTING HYDRANT W/ VALVE
- EXISTING UNDERGROUND ELECTRIC
- EXISTING OVERHEAD UTILITY

NOTES

1. THE CONTRACTOR SHALL VISIT THE SITE PRIOR TO SUBMITTING A BID TO BECOME FAMILIAR WITH ALL SITE CONDITIONS. ALL SURFACE FEATURES THAT CONFLICT WITH PROPOSED CONDITIONS SHALL BE REMOVED OR RELOCATED WHETHER OR NOT SHOWN ON SURVEY OR SPECIFICALLY CALLED OUT. THIS INCLUDES BUT IS NOT LIMITED TO CLEARING AND GRUBBING.
2. EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE INSTALLED PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. INSTALL EROSION AND SEDIMENTATION CONTROL PRIOR TO DEMOLITION.
3. CONTRACTOR SHALL COORDINATE WITH ALL SMALL UTILITY OWNERS FOR THE DEMOLITION AND RELOCATION OF EXISTING UTILITY STRUCTURES AND LINES AS NECESSARY TO ACCOMMODATE PROPOSED IMPROVEMENTS.
4. USE TEMPORARY CONSTRUCTION FENCING TO SECURE BUILDING SITE FOR PROTECTION OF PERSONS AND PROPERTY.
5. THE CONTRACTOR SHALL REFER TO THE MUTCD FOR TRAFFIC CONTROL REQUIREMENTS. THE CONTRACTOR MAY BE REQUIRED TO PHASE CONSTRUCTION TO MINIMIZE DISRUPTION TO TRAFFIC AND MAINTAIN SITE SAFETY. CONSTRUCTION OPERATIONS SHALL BE PERFORMED IN A WAY TO MINIMIZE THE DISRUPTION TO THE NORMAL FLOW OF TRAFFIC ON ALL STREETS, ALLEYS, AND PUBLIC BUILDING ACCESS.
6. THE CONTRACTOR SHALL OBTAIN AND CONFORM TO ALL PERTINENT CITY, COUNTY AND STATE DETAILS, SPECIFICATIONS, PERMITS, AND COORDINATE INSPECTIONS AS REQUIRED BY THE GOVERNING BODY DURING THE ENTIRE PROJECT DURATION.
7. CONTACT PUBLIC AND PRIVATE UTILITY OWNERS A MINIMUM OF 72 HOURS PRIOR TO THE START OF WORK. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND PROTECT ALL EXISTING UTILITIES AND TOPOGRAPHIC FEATURES NOT SCHEDULED FOR REMOVAL. NO ADDITIONAL PAYMENT WILL BE MADE FOR REPAIRING DAMAGE TO EXISTING UTILITIES, RELOCATING, WORKING AROUND, OR PROTECTING EXISTING UTILITIES OR OTHER APPURTENANCES.
8. A DIAMOND EDGE SAWBLADE SHALL BE USED FOR CUTTING ALL PAVEMENT MARKED FOR REMOVAL.
9. SAWCUT EXISTING PAVEMENT WHICH ABUTS ALL NEW PAVEMENTS TO PROVIDE A STRAIGHT VERTICAL EDGE.
10. WHERE NEW CONCRETE ABUTS EXISTING PAVEMENT, SAWCUT AND REMOVE PAVEMENT AT LEAST 1-FT FROM PROPOSED CONCRETE EDGE. REPLACE PAVEMENT WITH STRUCTURAL SECTION EQUAL TO OR GREATER THAN EXISTING SECTION UNLESS OTHERWISE NOTED.
11. PROTECT BY WHATEVER MEANS REQUIRED ALL FENCES, SIGNS, STRUCTURES, DRIVES, SIDEWALKS, STREETS, BUSHES, TREES, ETC. WHICH ARE NOT DESIGNATED FOR REMOVAL, OR OUTSIDE THE LIMITS OF CONSTRUCTION.
12. CONTRACTOR IS RESPONSIBLE FOR REPAIRING ALL DAMAGE INCURRED DURING CONSTRUCTION. THIS INCLUDES, BUT IS NOT LIMITED TO, DAMAGE CAUSED BY SUBCONTRACTORS TO THE GENERAL CONTRACTOR. REPAIRS SHALL EQUAL OR EXCEED THE QUALITY OF EXISTING CONDITIONS.
13. CONTRACTOR SHALL REMOVE AND/OR RELOCATE ALL ITEMS NECESSARY TO ACCOMMODATE THE PROPOSED IMPROVEMENTS WHETHER SPECIFICALLY CALLED OUT BY NOTE OR NOT.
14. THE LOCATIONS OF ALL AERIAL AND UNDERGROUND UTILITY FACILITIES ARE APPROXIMATE OR MAY NOT BE INDICATED IN THESE PLANS. UNDERGROUND FACILITIES, WHETHER INDICATED OR NOT, SHALL BE LOCATED PRIOR TO CONSTRUCTION. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING UTILITIES, PAVEMENT AND OTHER IMPROVEMENTS. ANY DAMAGE TO EXISTING UTILITIES AND/OR PAVED STREETS CAUSED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR EXPENSE.
15. ALL DEBRIS AND REFUSE RESULTING FROM CONSTRUCTION OPERATIONS SHALL BE HAULED OFF-SITE AND DISPOSED OF PROPERLY AT THE CONTRACTOR'S EXPENSE

GRADING NOTES:

1. THE PROPOSED GRADES SHOWN ON THE GRADING PLAN ARE FINISHED GRADES. SPOT ELEVATIONS ALONG CURB LINES REPRESENT THE FLOWLINE UNLESS OTHERWISE NOTED. THEY DO NOT ACCOUNT FOR THE 2" SUMP FOR THE RIM SHOWN ON THE UTILITY PLAN
2. SIDEWALK CROSS-SLOPES SHALL NOT EXCEED 2.0% AND LONGITUDINAL SLOPES SHALL NOT EXCEED 5.0%
3. THE CONTRACTOR SHALL ADJUST TO GRADE ALL MANHOLE STRUCTURES AND APPURTENANCES THAT FALL WITHIN THE LIMITS OF THIS CONTRACT. THE CONTRACTOR SHALL KEEP ALL SAID EXISTING UTILITIES AND THEIR APPURTENANCES FREE OF DEBRIS AND OPERABLE AT ALL TIMES DURING CONSTRUCTION.
4. ALL GRADES WITHIN THE LANDSCAPED AREA SHALL NOT EXCEED 4 HORIZONTAL TO 1 VERTICAL AND HAVE A MINIMUM GRADE OF 2%, UNLESS OTHERWISE NOTED.
5. ALL PAVEMENT GRADES SHALL BE AS FOLLOWS IN THE DIRECTION OF FLOW:
 - ASPHALT PAVEMENT - 1.5% MINIMUM, 5% MAXIMUM
 - CONCRETE PAVEMENT - 1% MINIMUM, 5% MAXIMUM
 - CONCRETE CURB AND GUTTER - 0.50% MINIMUM
6. SLOPES GREATER THAN 4:1 ARE STABLE FROM LAND SLIDING AND SURFACE EROSION.

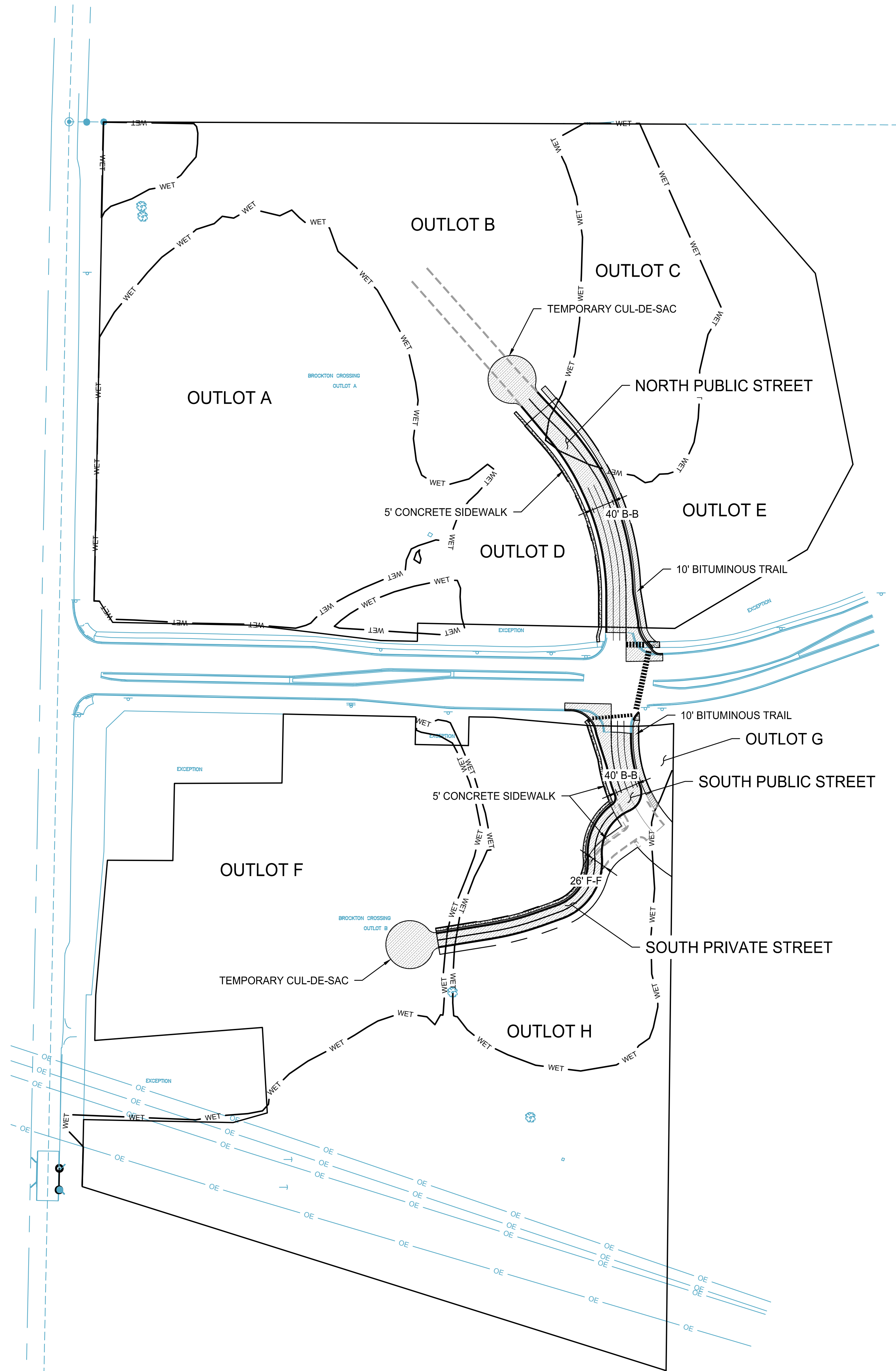
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PLAN BY: CHECK BY: PS
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| REVISIONS | | NO. | DATE | DESCRIPTION |
|-----------|--|-----|-----------|------------------------|
| | | 1 | 7/19/2024 | Revised City Submittal |
| | | 2 | 8/22/2024 | Revised City Submittal |
| | | 3 | 9/25/2024 | Revised City Submittal |

OVERALL
EXISTING
CONDITIONS

DAYTON CREEK ADDITION
SCHANY GROUP, LLC

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LEGEND

| | |
|--|------------------------------------|
| | PROPOSED CURB AND GUTTER |
| | PROPOSED BITUMINOUS (EDGE) |
| | PROPOSED CONCRETE (EDGE) |
| | APPROX. CONSTRUCTION LIMITS |
| | PROPOSED BITUMINOUS ROAD/ TRAIL |
| | PROPOSED BITUMINOUS DRIVEWAY/ WALK |
| | PROPOSED CONCRETE DRIVEWAY/ WALK |
| | PROPOSED TRUNCATED DOMES |

SITE PLAN NOTES

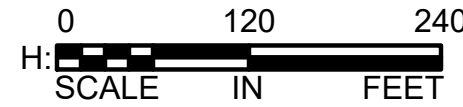
- ALL LOT DIMENSIONS ARE ROUNDED TO THE NEAREST FOOT.
- ALL AREAS ARE ROUNDED TO THE NEAREST SQUARE FOOT.
- STREET NAMES ARE SUBJECT TO APPROVAL BY THE CITY.
- DRAINAGE AND UTILITY EASEMENTS SHALL BE PROVIDED AS REQUIRED. DRAINAGE AND UTILITY EASEMENTS WILL BE PROVIDED OVER ALL PUBLIC UTILITIES AND UP TO 1 FOOT ABOVE THE HIGH WATER LEVEL OF ALL PONDS.
- STREET WIDTHS ARE SHOWN FROM FACE OF CURB TO FACE OF CURB (F-F) , OR BACK OF CURB TO BACK OF CURB (B-B) (SEE TYPICAL SECTIONS AND CITY DETAILS STR-06 AND STR-07).
- THE CONTRACTOR SHALL CONSTRUCT ALL PAVEMENTS TO CONFORM WITH THE CORRECT LINES AND FINISHED GRADES AS INDICATED ON THE PLANS AND TO MATCH EXISTING PAVEMENT GRADES AT TIE-IN POINTS. NO PONDING OF WATER WILL BE ALLOWED.
- SAW ALL CONCRETE CONSTRUCTION JOINTS, CLEAN THEM OF DEBRIS, BLOW THEM DRY AND IMMEDIATELY SEAL WITH JOINT SEALANT.
- REINFORCE ODD SHAPED PAVING PANELS WITH #3 BARS AT 24" EACH WAY. AN ODD SHAPED PANEL IS CONSIDERED TO BE ONE IN WHICH THE SLAB TAPERS TO A SHARP ANGLE WHEN THE LENGTH TO WIDTH RATIO EXCEEDS 3 TO 1, OR WHEN A SLAB IS NEITHER SQUARE NOR RECTANGULAR.

SITE DEVELOPMENT DATA

| | |
|--------------------------|--------------------|
| EXISTING ZONING | A-1 - AGRICULTURAL |
| PROPOSED ZONING | PUD |
| GROSS SITE AREA: | 51.88 AC |
| NORTH PUBLIC STREET ROW: | 0.82 AC |
| SOUTH PUBLIC STREET ROW: | 0.49 AC |
| PRIVATE STREET: | 0.38 AC |
| POWER EASEMENT | 6.17 AC |
| NET SITE AREA: | 44.02 AC |
| PONDING / OPEN SPACE: | 22.29 AC |
| DEVELOPMENT SUMMARY: | |
| DEVELOPMENT SITES | 4 LOTS |
| DEVELOPABLE AREA | 21.73 AC |

OUTLOT TABLE

| OUTLOT | USE | GROSS AREA |
|--------|--|------------|
| A | WETLAND/OPEN SPACE | 522,665 SF |
| B | FUTURE DEVELOPMENT SITE | 247,193 SF |
| C | WETLAND/ OPEN SPACE | 149,681 SF |
| D | FUTURE DEVELOPMENT SITE | 71,111 SF |
| E | FUTURE DEVELOPMENT SITE | 213,294 SF |
| F | FUTURE DEVELOPMENT SITE | 415,082 SF |
| G | OPEN SPACE | 8,630 SF |
| H | PRIVATE ROAD/ WETLAND/ PONDING/ POWER EASEMENT | 575,373 SF |



REVISIONS

| NO. | DATE | DESCRIPTION |
|-----|-----------|------------------------|
| 1 | 7/19/2024 | Revised City Submittal |
| 2 | 8/22/2024 | Revised City Submittal |
| 3 | 9/25/2024 | Revised City Submittal |

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

VICKI J. VAN DELL
DATE: 08/22/2024 LIC. NO. 41352

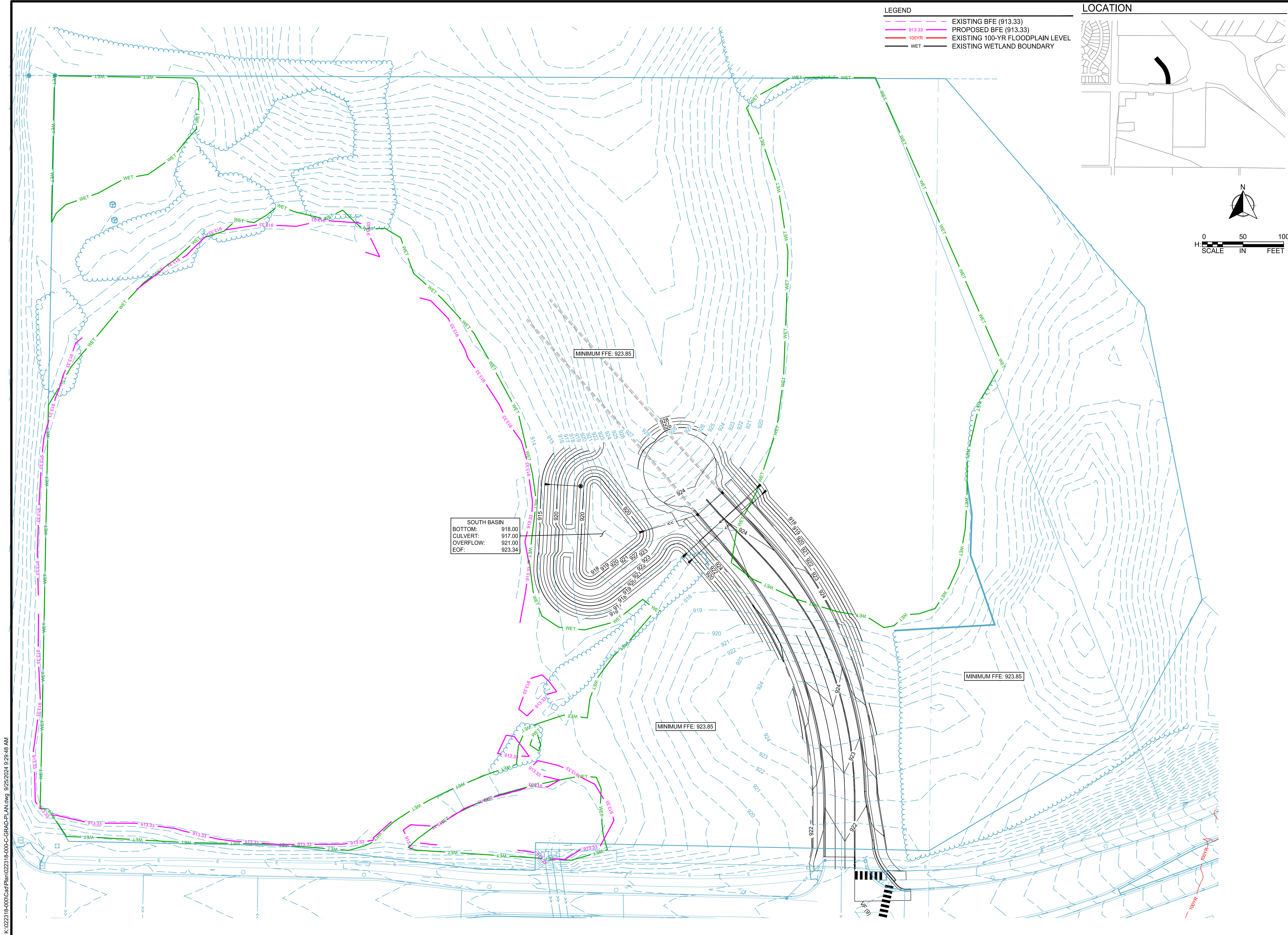
PRELIMINARY
SITE PLAN

DAYTON CREEK ADDITION
SCHANY GROUP, LLC

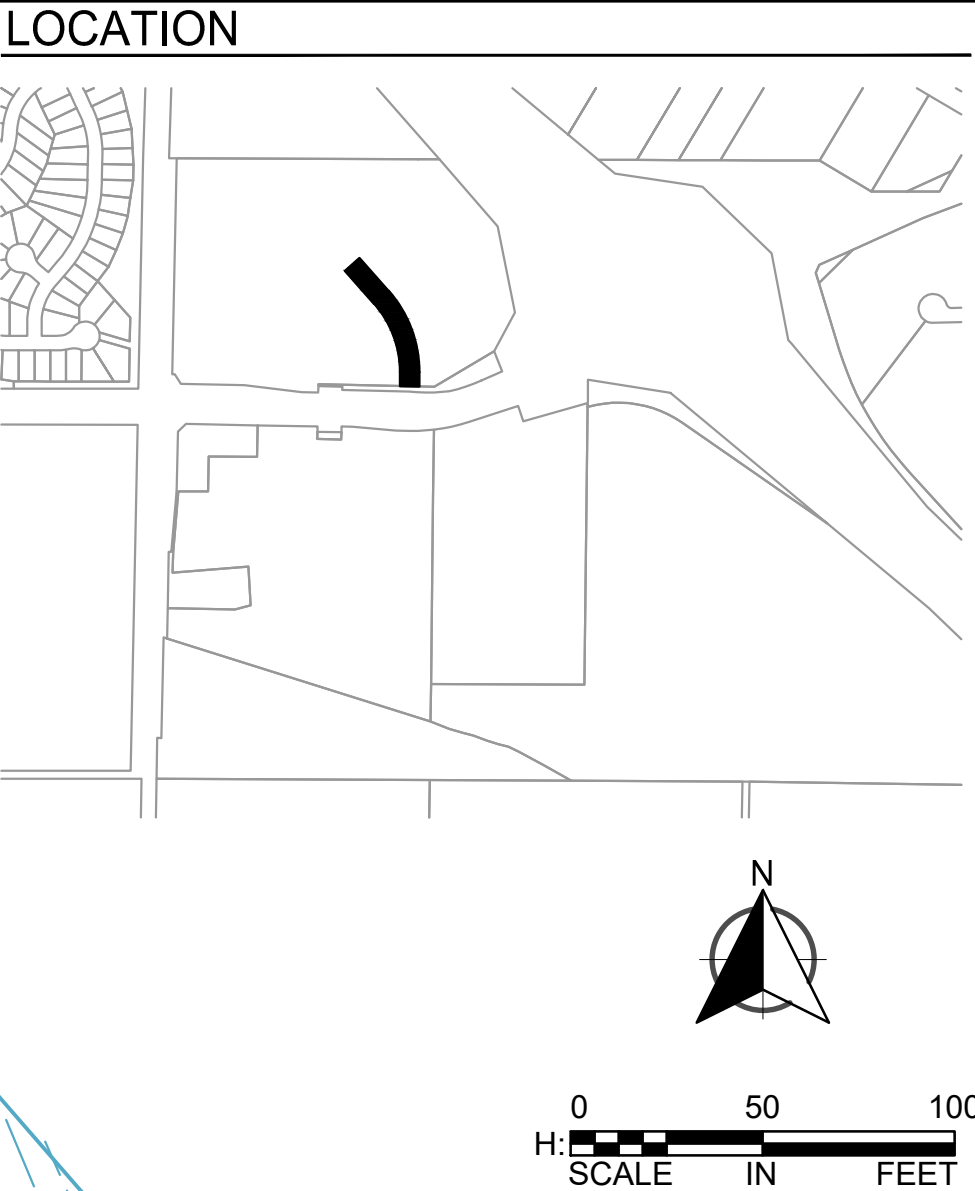
WSB PROJECT NO.
022318-000

SHEET
19 OF 33

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- LEGEND
- EXISTING BFE (913.33)
 - PROPOSED BFE (913.33)
 - EXISTING 100-YR FLOODPLAIN LEVEL
 - EXISTING WETLAND BOUNDARY



REVISIONS

| NO. | DATE | DESCRIPTION |
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VICKI J. VAN DELL
DATE: 08/22/2024 LIC. NO.: 41352

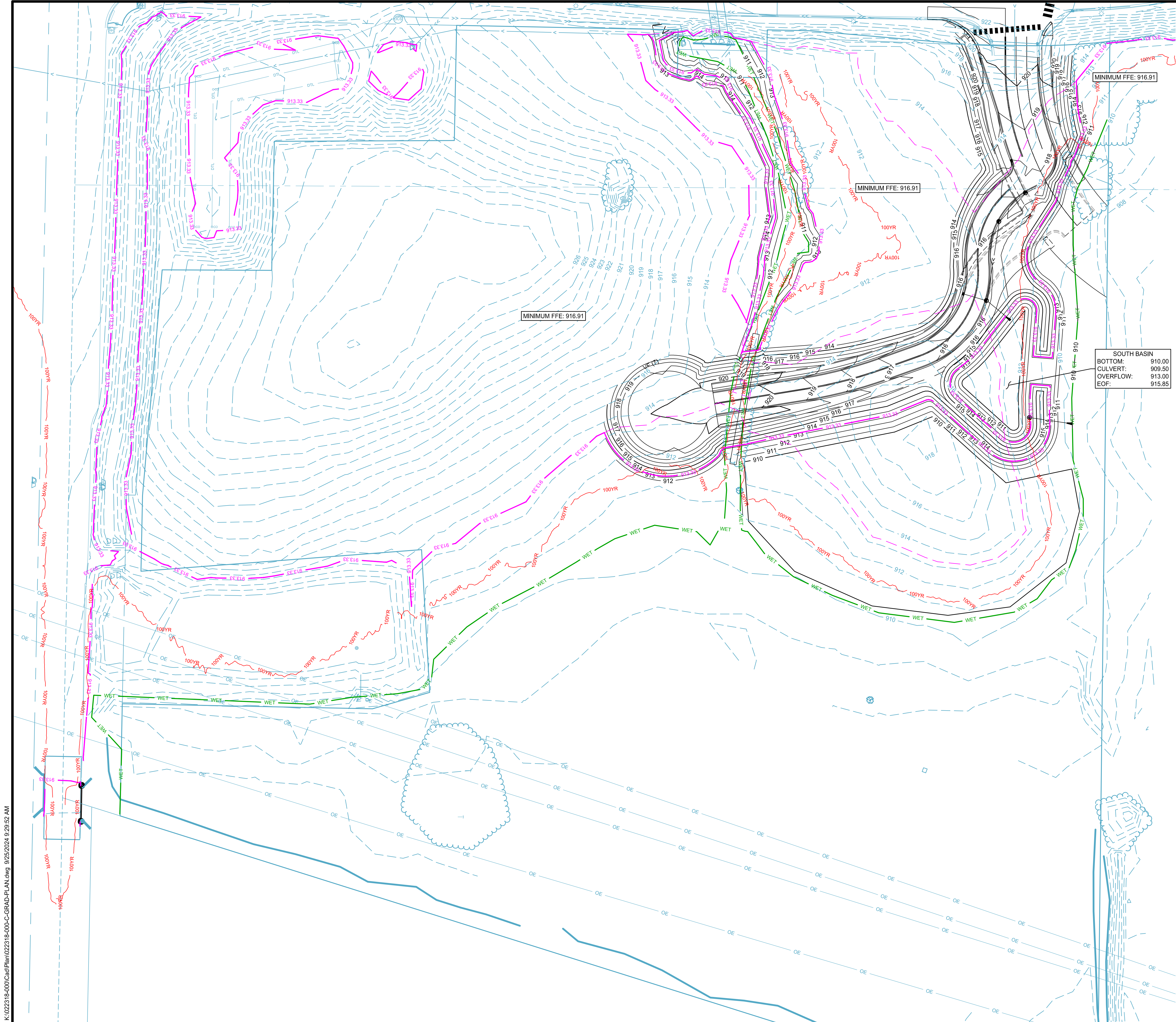
NORTH
GRADING PLAN

DAYTON CREEK ADDITION
SCHANY GROUP, LLC

WSB PROJECT NO.
022318-000

SHEET
32 OF 33

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LOCATION

0 50 100
SCALE IN FEET

LEGEND

- EXISTING BFE (913.33)
- PROPOSED BFE (913.33)
- EXISTING 100-YR FLOODPLAIN LEVEL
- EXISTING WETLAND BOUNDARY

SOUTH BASIN
BOTTOM: 910.00
CULVERT: 909.50
OVERFLOW: 913.00
EOF: 915.85

wsb

SCALE: AS SHOWN
DESIGN BY: KH
PLAN BY: IE
CHECK BY: PS

| NO. | DATE | DESCRIPTION |
|-----|-----------|------------------------|
| 1 | 7/19/2024 | Revised City Submittal |
| 2 | 8/22/2024 | Revised City Submittal |
| 3 | 9/25/2024 | Revised City Submittal |

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VICKI J. VAN DELL
DATE: 08/22/2024 LIC. NO. 41352

SOUTH GRADING PLAN

DAYTON CREEK ADDITION
SCHANY GROUP, LLC

ITEM:

Resolution Finding that the Acquisition of certain property by the City of Dayton is in compliance with the City's Comprehensive Plan

APPLICANT/PRESENTERS:

City of Dayton

PREPARED BY:

Jon Sevald, Community Development Director

BACKGROUND/OVERVIEW:

In 2024, the city accepted a Fire Services study, identifying the need to eventually replace Fire Stations 1 & 2, providing optimal locations for maximum coverage within an 8-minute response. These locations are; (1) North Diamond Lake Road & Xanthus Lane, and (2) Fernbrook Road & 129th Avenue.¹ Neither location currently has access to sewer and water and are in the 2040 and Post 2050 Staging areas.

In 2028, the city will take delivery of an aerial ladder truck. Prior to delivery, a new Fire Station is needed (Station 3). The ladder truck will not fit in Station 1 or 2 without displacing other vehicles. During the May 28, 2024 City Council meeting, the Council considered acquiring a 6.5 acre tax forfeit property located on Dayton Parkway and Territorial Road for Station 3 and a fire training facility. The Council denied the acquisition, (summarized) that a fire station is not the highest and best use of this property. Staff was directed to pursue alternative locations.

The city has negotiated the purchase of a 9.5 acre property near 117th Avenue & East French Lake Road. Prior to purchasing or selling real property, the Planning Commission must review and make written findings as to compliance with the Comprehensive Plan.²

The property consists of farmland and woods. The property is zoned A-1 Agricultural, and is guided Commercial, Neighborhood Commercial, and Greenway Overlay in the 2040 Comprehensive Plan.³ Additionally, the property is within the Master Plan area, guided Medium Density Residential.⁴

The Comprehensive Plan guides public facilities as *Public/Institutional* and does not identify any property for *future* public facilities, other than future "park and open space". Staff's intent is to complete a facility needs analysis in 2026, and to pursue a Comprehensive Plan amendment, and Zoning Map amendment at the time the station is built (2026-2027).

CRITICAL ISSUES:

- Determine if acquisition of this property for a government use is compliant with the Comprehensive Plan.

¹ City of Dayton Fire Department Master Plan, May 2024, p.15

² MN Statute 462.356, Subd 2 (Compliance with plan)

³ 2040 Comprehensive Plan, Fure 3: Future Land Use Map

⁴ DRAFT Dayton Parkway Master Plan, February 2024

PLANNING COMMISSION MEETING

60/120-DAY RULE (IF APPLICABLE):

| | 60-Days | 120-Days |
|-------|---------|----------|
| (N/A) | (date) | (date) |

RELATIONSHIP TO COUNCIL GOALS:

Build Quality Infrastructure

- Address Current Facility Needs

Planning Ahead to Manage Thoughtful Development

- Future Planning for City Facilities and Personnel
- Complete Large Area Plans for Next Staging Area

ROLE OF PLANNING COMMISSION:

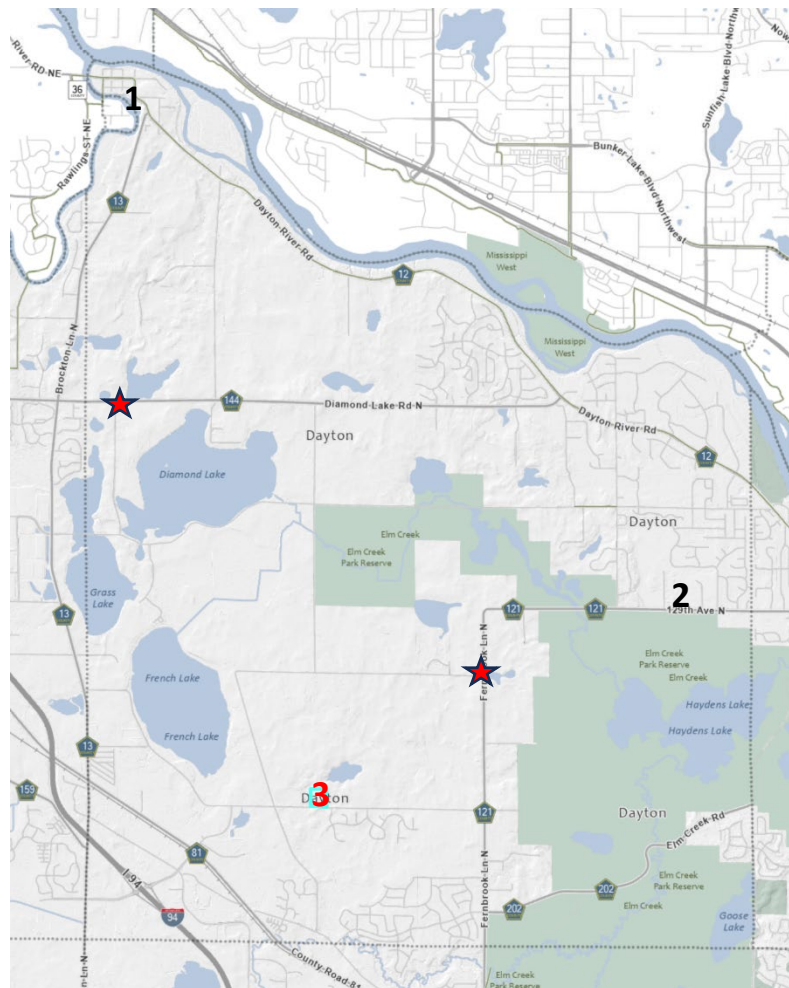
Provide written findings supporting or denying compliance with the Comprehensive Plan.

RECOMMENDATION:

Staff recommends approval.

ATTACHMENT(S):

Aerial Map
Future Land Use Map
Site Photo
Resolution

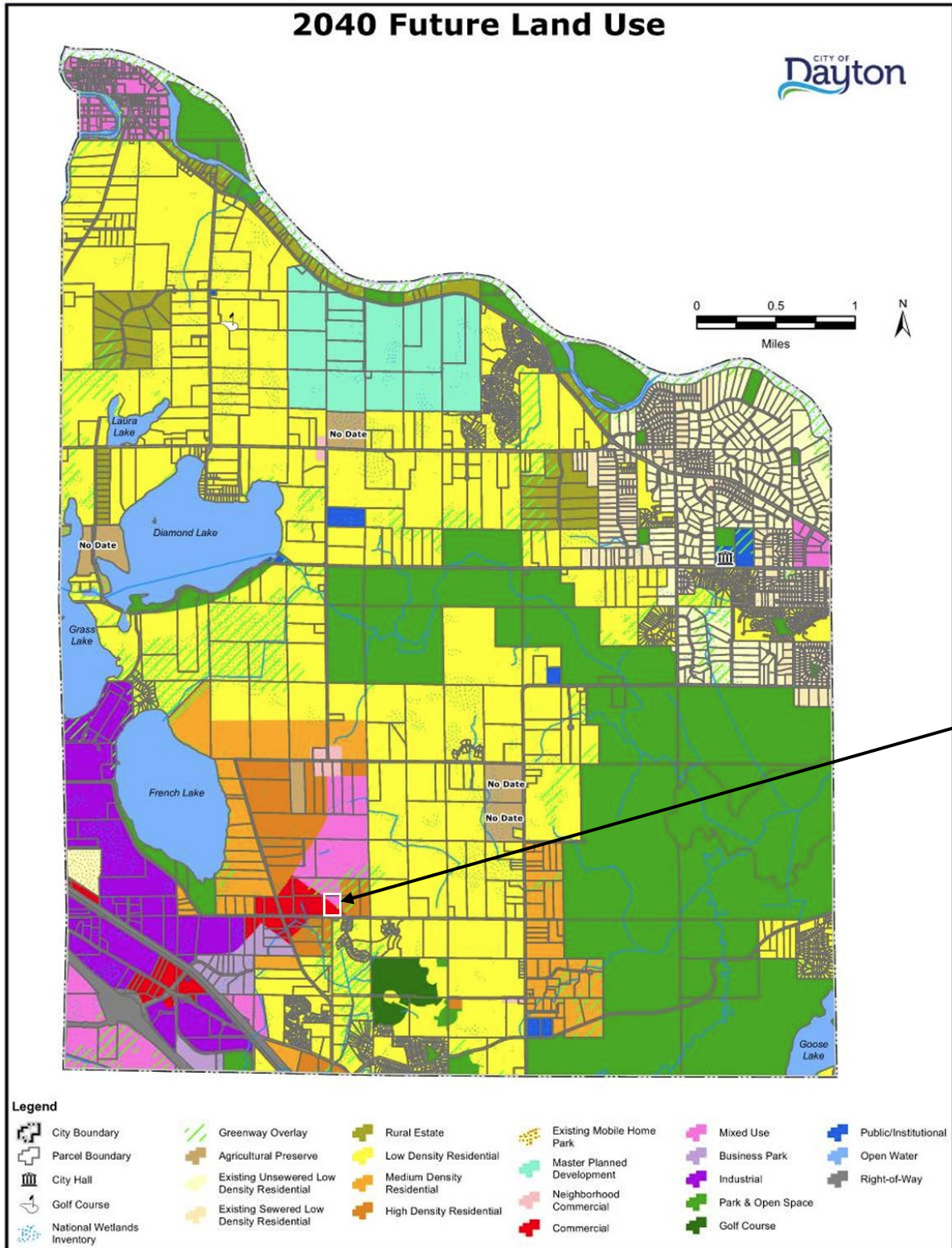


Location of Existing Station 1 & 2, and proposed Station 3. Stars = "optimal" locations if replacing Stations 1 & 2.

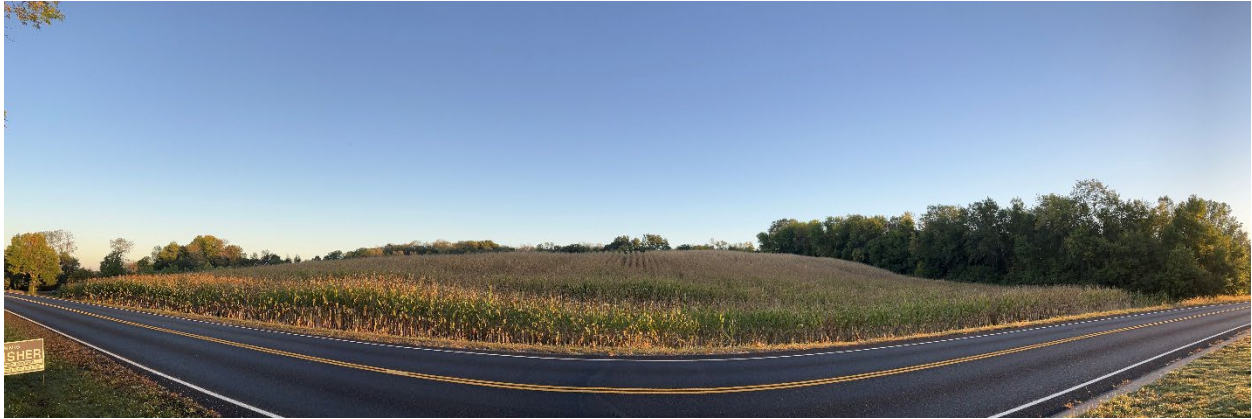


FUTURE LAND USE MAP

Figure 3: Future Land Use Map



SITE PHOTO



Panoramic view, looking north from 117th Ave. (Sep 26, 2024)



View looking northwest from 117th Ave. (Sep 26, 2024)

**CITY OF DAYTON
HENNEPIN AND WRIGHT COUNTIES, MINNESOTA**

**PLANNING COMMISSION
RESOLUTION NO. 01-2024**

**A RESOLUTION FINDING THAT THE ACQUISITION
OF CERTAIN PROPERTY BY THE CITY OF DAYTON
IS IN COMPLIANCE WITH THE CITY'S COMPREHENSIVE PLAN**

WHEREAS, the City Council of the City of Dayton ("City") has proposed to acquire certain real property legally described as follows:

PID: 29-120-22-44-0002

Lot 2, Block 1, Baxter Estates

WHEREAS, the proposed acquisition of the Property has been submitted to the Planning Commission for its review and issuance of a written report of its findings pursuant to Minnesota Statutes § 462.356, subd. 2; and

WHEREAS, the Planning Commission has reviewed the proposed acquisition of the Property as it relates to the City's Comprehensive Plan.

NOW, THEREFORE, BE IT RESOLVED, by the Planning Commission of the City of Dayton, Minnesota, that acquisition of the Property is found to be consistent with the City's Comprehensive Plan.

ADOPTED this 3rd day of October, 2024, by the Planning Commission of the City of Dayton, Minnesota.

DAYTON PLANNING COMMISSION

By: _____
Peter DeMuth, Chair

ATTEST:

Secretary

ITEM:

Discussion of Metropolitan Council, Imagine 2050

APPLICANT/PRESENTERS:

City of Dayton

PREPARED BY:

Jon Sevald, Community Development Director

BACKGROUND/OVERVIEW:

The Metcouncil is the regional planning authority for the seven-county metro. Every ten years, the Metcouncil updates its policy plans for Land Use, Parks & Trails, Housing, Water, and Transportation. Draft "Imagine 2050" was released in Mid-August. Public Comment period closes October 7th. The Metcouncil will approve Policy Plans in February 2025, and release System Statements to local governments in fall, 2025. The System Statements become the basis for Dayton's Comprehensive Plan update, due in 2028.

The Draft Imagine 2050 and supporting information may be found at <https://engage.metrocouncil.org/>. The Metcouncil held a Public Hearing on September 25, 2024. Twelve people testified, including representatives of the cities of Oak Grove, Andover, Sun Fish Lake, and Centerville regarding frustration with change, requests for exemptions, and explanations of the limitations of small cities have with comprehensive planning. Other testifiers represented special interests. Point being, there are many needs and conflicting values which Imagine 2050 is intended to balance.

The City Council discussed Imagine 2050 at its September 24, 2024 meeting, directing information to be posted on social media, and directed the Planning Commission to provide comments. The City Council's discussion noted contradictory policies, such as requiring higher density results in more impact to the environment.

A summary of the 1,123 page plan may be watched on YouTube at <https://www.youtube.com/watch?v=YxowNnYRurA> (1 hour 10 minutes). This is the quickest means to find out more information, but brief enough to not learn any details.

Staff has skimmed Imagine 2050 related to Land Use, Housing, Transportation, and Water. Two takeaways; (1) Metcouncil is not going to remove Dayton (or portions of) from the Metropolitan Service Area (MUSA), and (2) Net Density will increase from a minimum of 3 units per acre, to 4 units per acre.

CRITICAL ISSUES:

Dayton's Net Density of new residential developments fluctuates year to year:

| 2016 | 2017 | 2018 | 2019 | 2020 | 2021 | 2022 | 2023 |
|------|------|------|------|------|------|------|------|
| 3.18 | 2.38 | 2.99 | 3.9 | 3.04 | 3.70 | 3.0 | 0.66 |

PLANNING COMMISSION MEETING

2023 Net Density was likely mis-calculated, but impacted by large lot plats (River Overlook, Whispering Woods). 2021 includes Balam Pointe apartments (9-units per acre).

If Dayton is required to guide land for an average of 4-units per acre, this will require more land to be guided for townhomes and apartments. If the City's expectation is to continue current development patterns (predominantly single-family), the city will struggle to meet 4-unit density.

Considering the DRAFT Master Plan guides about 400-acres at 6-15 unit density, requiring a minimum of 4-units per acre shouldn't be problematic, assuming Southwest Dayton is developed at medium-high density, and the Master Plan area is moved to the 2030 and 2040 Staging Areas.

The city and Metcouncil discussed the city's intent to allow unsewered residential development less than 1:10 density, which the Metcouncil will not allow. Discussion continued for the potential of 4:40 density, which is allowed in communities guided "Diversified Rural" (not Dayton).¹ Further conversations are intended.

60/120-DAY RULE (IF APPLICABLE):

| | 60-Days (date) | 120-Days (date) |
|-----|-------------------|--------------------|
| N/A | | |

RELATIONSHIP TO COUNCIL GOALS:

Build Quality Infrastructure
Planning Ahead to Manage Thoughtful Development
Preserving our Rural Character
Create a Sought After Community

ROLE OF PLANNING COMMISSION:

Provide direction.

RECOMMENDATION:

Staff's recommendation is for the city to request that Metcouncil designate portions of northwest Dayton as "Diversified Rural" allowing 4:40 net density. The intent is to permanently establish a greenbelt around the Historic Village thereby preserving the area's rural character and providing a variety of housing choices throughout the city.

ATTACHMENT(S):

N/A

¹ August 9, 2024, attended by Mayor and city staff, District 1 Metcouncil member, and Metcouncil staff.